



## Package C136 - Farringdon Station

Level II Historic Building Record Comprising:

No. 3 Lindsey Street, Nos. 8-9 Hayne Street, Nos. 20-23 Long Lane, No. 2 Lindsey Street, Nos. 54-64 Charterhouse Street, Chimney to the rear of No. 20 Long Lane and retaining arches below Nos. 8-9 Hayne Street and No. 23 Long Lane

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## 1 Introduction

### 1.1 Project Background

This document reports on the historic building recording of structures located within Smithfield, London as part of the Crossrail redevelopment of the area. The site comprises an area bounded to the north by Charterhouse Street, to the east by Hayne Street, to the south by Long Lane and to the West by Lindsey Street (Appendix 2, Drawing C136-SWN-T-DDA-M123\_Z-25005). As part of the works authorised by the Crossrail Act 2008, these structures were identified for demolition to allow the construction of the Farringdon Eastern Ticket Hall.

Although not statutorily listed, a number of these structures were identified within the Site Specific Written Scheme of Investigation (C136-SWN-C2-JLT-M123-00001) as containing features of historic interest that warranted archaeological recording prior to their demolition. The recording work has been undertaken by Steven Handforth, Built Heritage Specialist at Scott Wilson Ltd. on behalf of Crossrail Limited (CRL).

Non-listed built heritage assessment and recording forms part of the archaeological mitigation strategy for Crossrail Limited (CRL). The definition of non-listed built heritage adopted follows Information Paper D22 Archaeology and encompasses above ground historic features and structural elements of historical interest.

### 1.2 Aims

The aim of the building recording is to provide an archaeological record of those structures identified as being of historic interest within the Site Specific Written Scheme of Investigation for Farringdon Station (C136-SWN-C2-JLT-M123-00001).

The principal objective of the work is to produce an illustrated written document in which the fabric, appearance and form of the buildings are detailed prior to their demolition. The interrelationship of the structures with the surrounding built form will be documented where appropriate. In addition to this, the document aims to identify significant archaeological features reflecting the form and function of the area at the various stages in its development.

This report documents the results of onsite analysis, comprising a photographic survey, supplemented by documentary research (comprising maps, photographs and other historical documentation held in various archives and libraries) in order to establish the historical context of the relevant parts of the site. The report interprets the findings of this photographic survey in light of the documentary research undertaken.

### 1.3 Methodology

The building recording was undertaken in accordance with a Level II record as defined in Understanding Historic Buildings: A Guide to Good Recording Practice (English Heritage, 2006).

An initial site inspection was undertaken in March 2010. At this point a number of sections of the buildings were inaccessible, either due to health and safety reasons or not visible due to the density of buildings obscuring historic fabric. As such, numerous site visits were undertaken prior to, and during the demolition works to allow access to all areas of the site, to provide a comprehensive understanding of the structures and their potential interrelationships. This work was undertaken between March 2010 and February 2011.

A written description of these buildings was produced using the photographic survey information and notes taken during on-site observation. This has identified features of archaeological and architectural interest, alongside evidence for the chronological and structural development of the area. As well as general photographs, close-up views were used to record any fine or complex details, typical and unusual features, signage, dates, mouldings, manufacturer's marks or other markings which contribute to an understanding of the structure's history, materials and their provenance.

The written record was supplemented by documentary and historical research comprising an examination of the relevant documentary (primary and secondary), pictorial and cartographic sources. These sources were critically examined and integrated into the overall interpretation of the site. Sources were consulted at a number of locations including the London Archaeological Archive and Research Centre (LAARC), The Greater London Industrial Archaeological Society (GLIAS), London Metropolitan Archives, The Bishopsgate Institute Library, The Centre for Metropolitan History (London's Past Online), British History Online and the Royal Institute of British Architects (RIBA).

The photographic survey was undertaken using both black and white 35mm prints and colour digital images. These images have been collated and indexed in accordance with the requirements of Management of Research Projects in the Historic Environment (MoRPHE, 2006). The contents of this archive will be submitted to the Greater London Historic Environment Record with additional copies deposited with the National Monuments Record (Swindon). In addition, an OASIS form will be filled out for the works.

Historic images are included within the text as figures whilst all photographs are located in Appendix 1 as plates. Drawings providing site location plan and photographic location plans are provided in Appendix 2. A photographic register for the black and white 35mm photography is attached in Appendix 3. The plans included within this document reflect the structures and their internal layouts at the time prior to soft strip, and are based on the asbestos survey undertaken by RPS in May 2010. Where photographs have been taken post demolition, they are highlighted within the photographic location plans in green for the sake of clarity.

### 1.4 Scope

The structures recorded within this building recording document include:

- Nos. 54-64 Charterhouse Square;
- Nos. 8-9 Hayne Street;
- Nos. 20-23 Long Lane;
- No. 3 Lindsey Street; and
- Chimney to the rear of Nos. 20-21 Long Lane and the retaining arches below Nos. 8-9 Hayne Street and No. 23 Long Lane.

Site location plans for these structures are included in Appendix 2 in Drawings 1-6.

Comprehensive internal inspections were made of all structures. General photographs were taken to provide the indicative appearance of each structure and their internal layout. Comprehensive photographic records were not undertaken in structures where rooms were either similar to one another or where the insertion of modern services and furnishing had removed all historic or architectural integrity. However, indicative examples of modern intrusion were provided in some instances to show the extent of the incremental erosion of internal services. One such example includes No. 20 Long Lane, which had lost all of its internal character, and as such only its exterior was recorded, alongside representative internal photographs.

The structure of the document, incorporating those features mentioned above is as follows:

- Section 2 provides a brief historical background and chronology of Smithfield as a whole to place the structures within their historic landscape context. This is followed by a detailed analysis of the chronological development of the site.
- Section 3 provides a descriptive and analytical record of each of the structures identified starting with Nos. 54-64 Charterhouse Street to the north and working clockwise, finally analysing the chimney to the centre of the site. The descriptions are based on on-site observations whilst the analysis is based on the findings from these observations in conjunction with the documentary research. The purpose

of the analysis is to aid the understanding of the significance of aspects of the site in terms of their function, architectural and historic interest prior to demolition.

## 2 Documentary Analysis

### 2.1 Historical background

This section provides an overview of the historical development of the area to provide a context in which the recorded buildings can be placed. It also details the evolution of the structures as a whole, providing information on individual structures and their relationship with one another.

### 2.2 Smithfield

Smithfield was used as a Roman Cemetery between the 1st and 4th centuries AD and prior to the Norman invasion was predominately made up of agricultural land. The settlement within the city walls expanded during the medieval period and the lack of space within the walls led to the foundation of monastic houses outside the City walls, particularly to the north in Clerkenwell and the Smithfield area. Four monastic houses were located in these areas: the Priory and Hospital of St Bartholomew founded in 1123; Charterhouse (1370); the priory/nunnery of St Mary Clerkenwell (c 1145); and the Priory of the Order of the Hospital of St John of Jerusalem, the headquarters of the Order in England, founded in 1144. From the Tudor period onwards the expanding population gradually led to the urbanisation of the remaining open areas outside the City walls (figure 1).



Figure 1 – Agas's map dating to 1562, the site is located to the bottom left of this image



Figure 2 – Faithorne Newcourt's map dating to 1658, the site is located just above 'Smith Field'

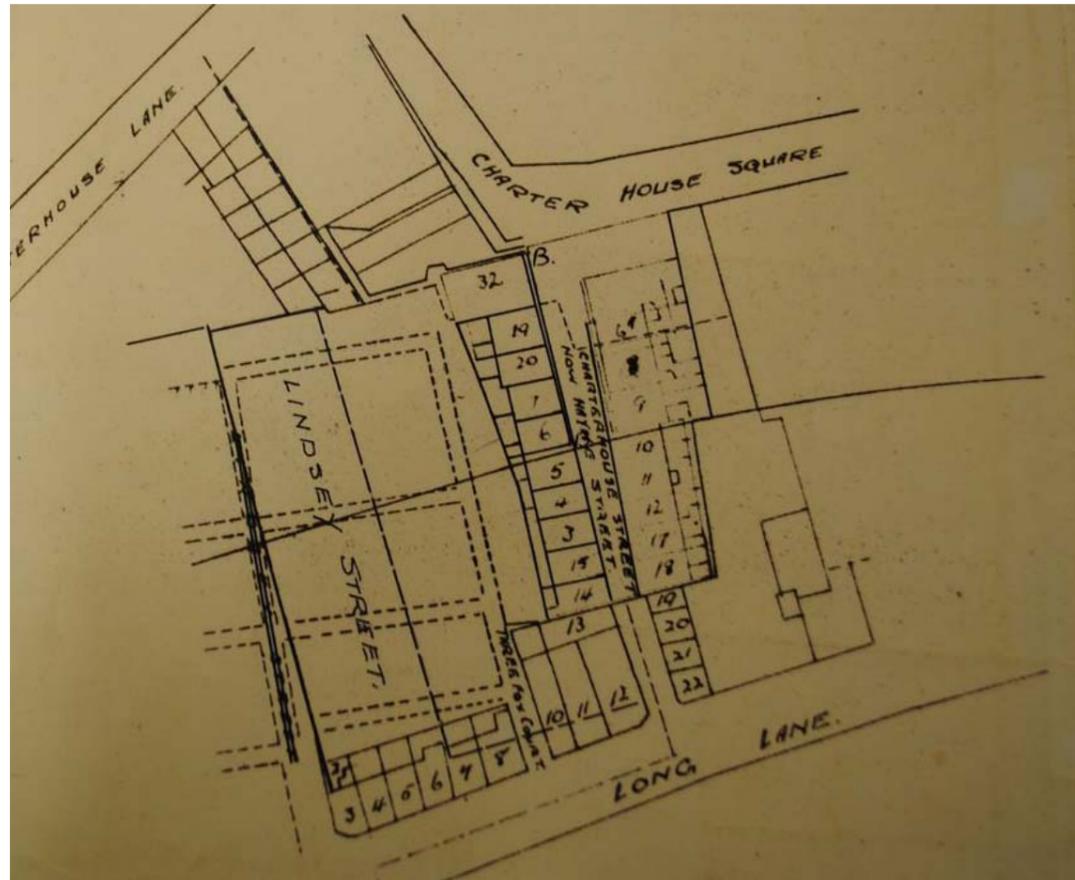
This density of built form is clearly apparent by the 17<sup>th</sup> century with a plethora of pitched roof timber framed structures lining the old medieval street pattern (figure 2). The trades that would have dominated this area are reflected in the names of the streets in the area including, Hosier Lane, Cloth Fair and Cock Lane.

Major changes occurred to the area in the Victorian period with the construction of the world's first underground line, the Metropolitan Railway which arrived at Farringdon Street Station in 1863. The line originally ran from Farringdon to Paddington, a distance of four miles (six km). The development of the rail infrastructure led to further developments including the construction of the grade II\* Smithfield Market by Horace Jones in 1868.

A series of arches were constructed under Smithfield market and the immediate vicinity to raise it above the railway and it is this distinct change in level that defines much of the built form of the structures described within this report.

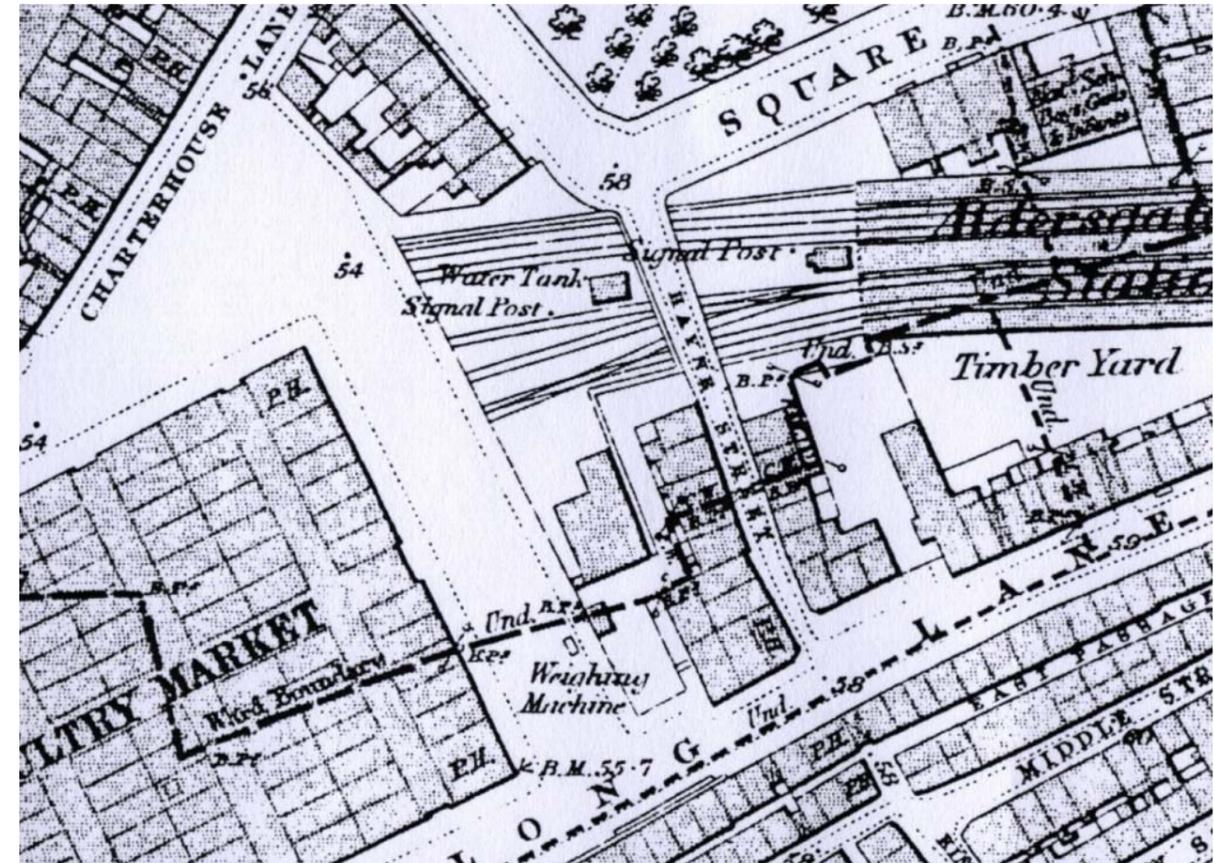
### 2.3 Development of the site

The development site itself has had a long and varied history. The earliest maps show the area was dominated by ad hoc development with narrow streets and dense housing developments (figure 2). The layout as it appears at the time of survey originates from the general improvements undertaken in the mid- to late 19<sup>th</sup> century with the construction of the railway, Holborn Viaduct and Smithfield Market. The site began to show signs of its current layout in the mid-19<sup>th</sup> century, with the construction of Hayne Street, named after its developer (Pevsner, 2002). These plans show a high density of structures lining either side of Hayne Street and Long Lane to the south (figure 3).



**Figure 3 – 1927 map, based on an earlier mid 19th century map (Source – LMA, GLC/AR/BR/22/BA/043620)**

The construction of the railway to the north led to a substantial number of these structures being demolished. New structures located within the site include a water tank and signal post as part of the railway operations and a weighing bridge to the south, probably used in conjunction with the operations of the market. The retaining wall can be clearly seen on this map showing it to be the earliest structure to be recorded within this document (figure 4).



**Figure 4 -1873 OS map**

Although Smithfield is predominately seen as an area dominated by the meat industry with butchers and frozen meat companies alongside the market, a number of trades are recorded within this site towards the end of the 19<sup>th</sup> century including bookbinders, printers, wine merchants, gold blockers, tailors and a temperance hotel (London Directories, 1895). The 1896 Ordnance Survey (OS) map depicts a number of the structures recorded in this document including No. 3 Lindsey Street, Nos. 8-9 Hayne Street (then 8-10 Hayne Street), Nos. 20-23 Long Lane and No. 2 Lindsey Street (figure 5). Railway sidings coming from Smithfield Market towards Nos. 8-9 Hayne Street are also visible on this map.

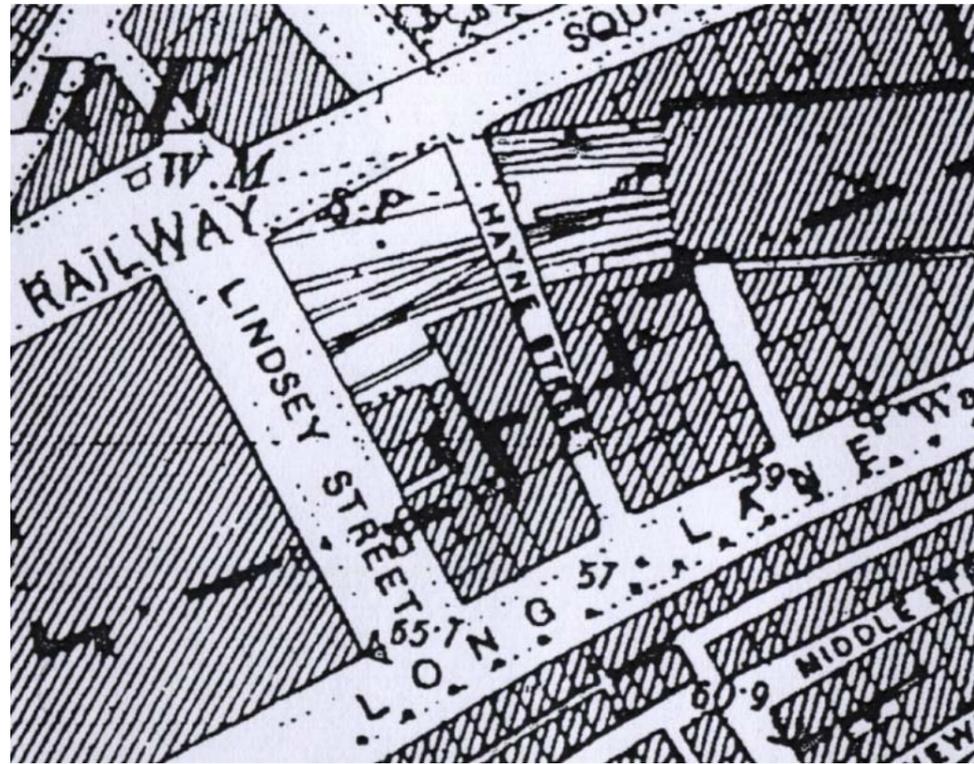


Figure 5 – 1896 OS map

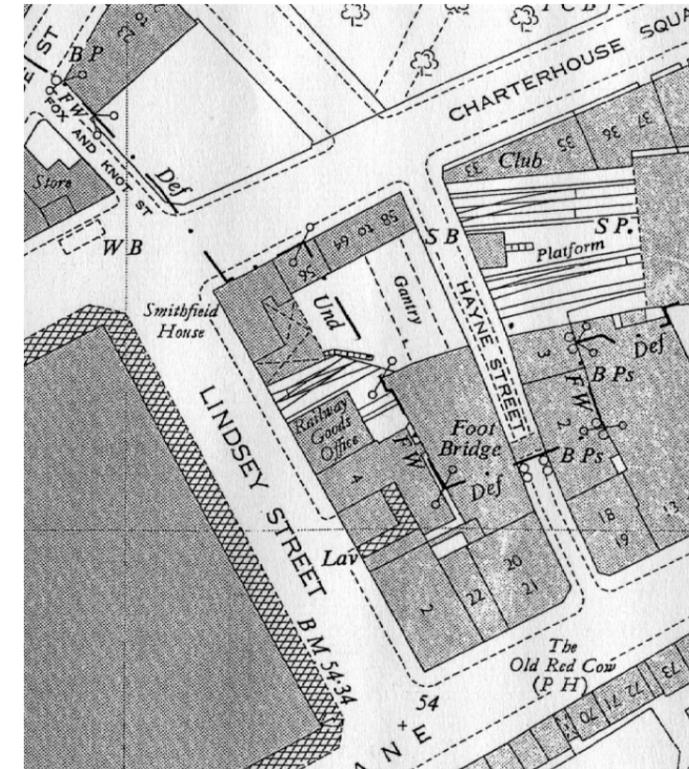


Figure 6 – 1953 OS map

By the mid-20<sup>th</sup> century, all the structures recorded within this report are extant with the construction of the steel raft on which Nos. 54-64 Charterhouse Street was constructed. Of additional interest is a gantry located between this structure and Nos. 8-10 Hayne Street as well as a series of other bridges linking the structures. Many of the buildings constructed here would have taken full advantage of their dual location at street level and at track level below. This was reflected in there being numerous railway goods offices located there and other similar industrial structures (figure 6)

### 3 Description and Analysis

The structures located within the site form an interesting, and highly complex remnant of the development of the area. Collectively, the structures form an important surviving example of the late 19<sup>th</sup> and early 20<sup>th</sup> century development of the Smithfield area. Their interest is predominantly of a townscape nature derived from their form, scale and massing as well as their historical interest with documentary and physical evidence of previous interrelationships between a number of the structures.

#### 3.1 Nos. 54-64 Charterhouse Street (Smithfield House)

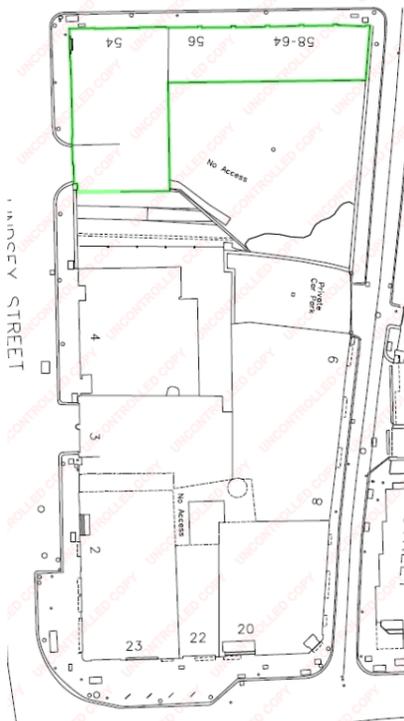


Figure 7 – 54-64 Charterhouse Street shown in green

##### 3.1.1 External Description

Of the structures recorded within this document, Nos. 54-64 Charterhouse Street is the most modern, being constructed in 1930 (figure 7). The structure is a loose interpretation of the Art Deco style and constructed using the Hennebique concrete technique. The structure's principal elevation fronts onto Charterhouse Street (plate 1). This façade is five bays wide, two storeys high with a shallow pitched pediment to the roofline with two smaller flanking pediments either side. The windows are all multi-paned casements located within shallow recesses. A set of coupled windows are located to the central three bays, whilst the outer windows are of a tripartite nature, the eastern one being taller to facilitate light into the stairwell (plate 2). To the east of the two-storey elevation is located a six bay single-storey blank façade with a variety of projecting and recessed panels divided by a horizontal string course towards its summit (plate 3).

The western elevation, facing onto Lindsey Street is seven bays long although the rhythm of the windows is disrupted by the varying widths between the narrow three windows to the south and the larger windows to the north (plate 4). As with the northern elevation, there is a variety of coupled and tripartite casement

windows, although the first and third bays from the north feature four light windows (plates 5-6). The ground-floor elevation features two large recesses, boarded up with timber panels at the time of survey. There is an access door to the southern opening and one to the north. The southern example appears to have been inserted into modern timber boarding which conceals a larger opening (plate 4). The structure features a relatively shallow slate roof.

The southern elevation presents a first storey blank white wall with pitched roof above, supported on three concrete pillars (plate 7). This suspended first-floor construction is continued around the eastern aspect of the structure to approximately five bays in length. The roof above is supported by a series of painted steel girders (plate 8). The eastern elevation features six apertures, one being a former opening to the south, now blocked (plates 7-8), one coupled window, one single width window and three large four light windows (plates 7-8). The northern section of the elevation which encloses the stairwell features no window hence the large window to the northern elevation (plates 2 and 8).

The single-storey block to the east features five principal entrance ways and seven smaller window openings. The entrances are large in their scale featuring no door which allows extensive light penetration in the building (plate 9). A number of these openings had been boarded up at the time of survey (plate 10).

##### 3.1.2 Internal Description

The structure is arranged in an L-Shape. The main body of the building, to the west, features two fairly plain rooms (plate 11). The access door to the northern elevation is large in scale and similar in its proportions to two loading doors on the eastern elevation, one of which is now partially boarded (plates 12, 14). These doors provide access to the yard to the rear. Opposite this, to the northwest of the room are two panelled doors, one with two over two and one with three over three panelling with strong vertical emphasis in their composition (plate 13). The ceiling features a matrix of exposed slender steel I-beam girders (plate 15).

To the north, the room fronting onto Charterhouse Street retains its original moulded picture rail (plate 16). A large doorway is located to the Lindsey Street elevation featuring six over six small lights above flanked by slender two over one horizontal apertures creating emphasis to the entrance (plates 17-18). The southeast section of this room is plain in appearance (plate 20) and features another doorway to a smaller room which has no visible historic fabric. Access to the upper storeys is provided by a modern doorway to the east which enters into a lobby area. This room was also accessible externally via two, two panelled doors with three over three glass panes above. Above this were a further seven over seven lights providing addition light into the lobby (plate 21).

A cantilevered doglegged concrete stairwell with steel banisters and plastic handrail provides direct access to the first floor (plates 21-22). The upper storey features a large open space with concrete floor and steel double 'W' trusses above (plates 23-25). The south elevation has been partially rendered whilst the upper section revealed Fletton brickwork masonry (plate 25). To the north of these are three separate smaller rooms with plain interiors and modern doors (plate 26).

The eastern single-storey block, contains three internal rooms, linked together by a long corridor to the south, used as an access between the rooms (plate 27). Various exposed soil pipes and services were visible in this area, reflecting the industrial character of the building. Apart from this, the rooms had no surviving historic features within them having been heavily modified since their construction (plates 28-29).

The demolition of Nos. 54-64 Charterhouse Street revealed the original steel decking structure which supported the building above the railway tracks below. The extensive nature of this composition can be seen in plate 30.

##### 3.1.3 Analysis

Smithfield House was constructed in 1930 on a steel and concrete raft over the train tracks below (plate 30). The architect for the structure was Mr. S.A.S Yeo, who was also responsible for the Ironbridge Tavern (now Inner London Hotel) in East India Dock Road (Hobhouse, 1994). The fact that such lengths were undertaken to provide additional structures near to Smithfield Market reflects the profitability of such a central location. A

selection of plans held by the London Metropolitan Archives (LMA) reveal the original function and composition of the structure. (figures 8-10).



Figure 8 – 1930 plan showing historic use of the site as workshops and office. (Source – LMA, GLC/AR/BR/22/BA/043620)

It is clear from these sources that the structure was originally used as workshops, with the wing to the east and the majority of rooms in the western range being predominately used for industrial purposes (figures 8-9).

Figure 8 shows that the single-storey block was originally divided into six spaces; whilst the south room in the two storey section was divided into two (figures 8-9). Evidence in the form of scarring for this was clear in the single storey block (plate 29) but not in the southern range due to the internal walls being painted. The large room fronting onto Charterhouse Street was used originally as the office, having the most important room located on the most important façade. This is reflected in the more elaborate entrance doorway and moulded picture rail that survive internally (plates 16-18).

The original plans also suggest that the workshops on the ground floor of the two storey building were originally open on two sides, presumably for the ease of transporting goods from the yard, to the rear to Lindsey Street. A main carriage way is also identified with railings to the southern most extent (figures 8-10).

Plans and records also show that there was a ten tonne travelling crane gantry constructed which was fixed to the single-storey structure and to No. 10 Hayne Street (figure 9). No evidence for this remains due to the demolition of 10 Hayne Street and painted elevation of the single-storey structure (plate 10). The gantry was presumably used either to delivery heavy goods into or out of the workshop areas.

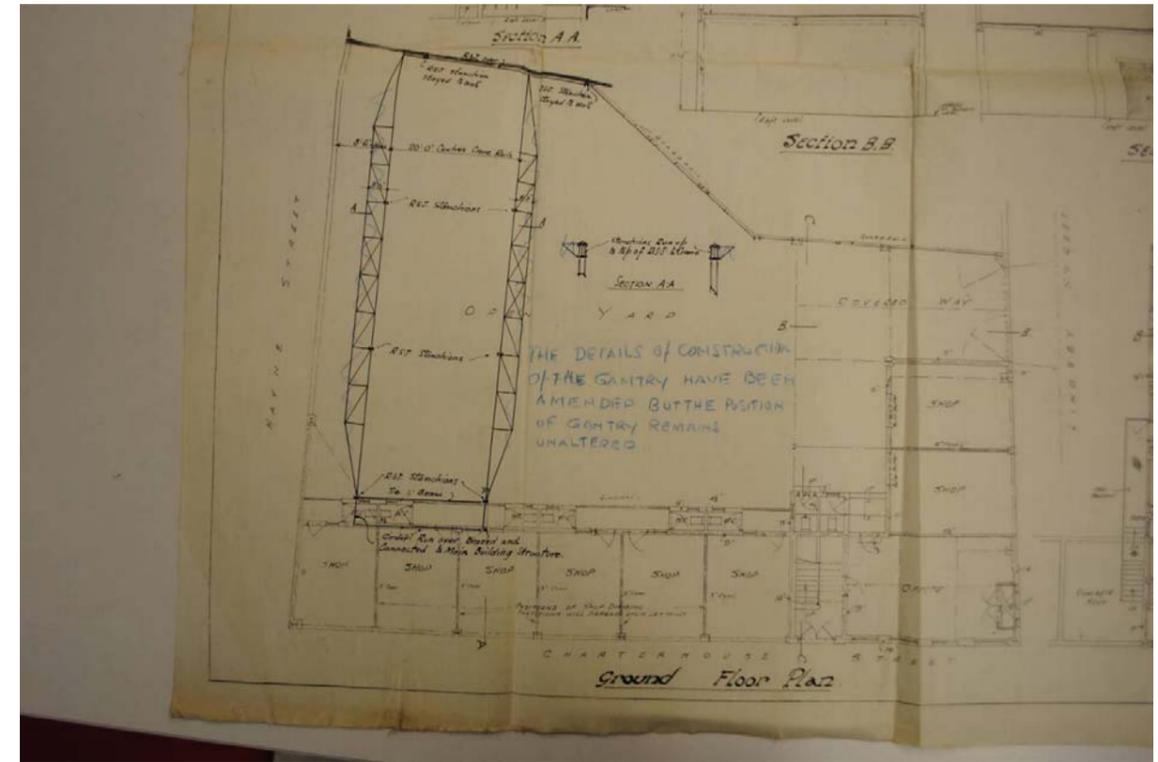


Figure 9 – 1930 plan showing evidence of the interrelationship between the structure and 8-10 Hayne Street with a girder between the two structures. (Source – LMA, GLC/AR/BR/22/BA/043620)



Figure 10 –1930 drawing showing Lindsey Street elevation (Source – LMA, GLC/AR/BR/22/BA/043620)

### 3.2 8-9 Hayne Street



Figure 11 – 8-9 Hayne Street shown in green

#### 3.2.1 External Description

8-9 Hayne Street (figure 11) is a late Victorian warehouse structure, seven bays in width, four storeys high with additional attic storey above, concealed behind a parapet roof (plates 31-32). The building is constructed of stock brick with a mixture of coupled windows divided by engaged colonettes with two over two casement windows to the six bays to the north, and a single one over one casements to the first bay to the south (plates 33-34). All windows are of a segmental variety with red brick voussoirs above. This rhythm is maintained throughout the first, second and third storeys. Below the first floor is located a continuous horizontal steel I-beam, positioned above five timber windows with stone hood moulds. Two modern doorways to the second and sixth bays (from south to north) were also discerned. Attached to this façade are also three cast iron plaques, one of which is a Finsbury boundary marker (plate 37).

The northern elevation presents a plain, stock brick elevation, painted white to the ground floor and showing scarring from previous openings and structures attached to the wall (plates 35-36). There is also a clear colour change of brick to the attic storey, suggesting that the attic storey was a later addition (plate 36).

The rear façade is far plainer, featuring nine bays with five bays of one over one coupled windows to the north, a single bay with a one over one sash window towards the south and three smaller one over one sash windows to the south, all located on the first and third storeys (plates 38 and 40). All the windows here are topped by segmental brick arches. This elevation also clearly shows the inserted attic storey with its five

coupled modern dormer windows, with the more modern clean stock bricks providing a contrast with the soiled original bricks below (plates 38-39). To the south of the structure an external rectangular structure with blocked doorway is also visible (plate 39). The ground-floor level features a lean-to structure with a half pitched glass roof (plate 38).

#### 3.2.2 Internal Description

Much of the original layout of each floor of the structure has been altered to suit modern requirements. Each floor is accessed from the south of the structure via a modern stairwell. Upon entering each level, modern services and suspended ceilings have obscured much of the original fabric. The main item of note within each floor are the regularly spaced cast iron columns coupled with a high number of original windows of differing construction.

The basement area comprises a large central space with numerous inserted modern partitioned rooms (plates 41-42). Visible within the space are a series of painted squat cast iron columns with an astragal moulding near their summit and other plain mouldings to the capital (plate 43). These columns are located in two rows running north to south along the floor. The area is lit to the east by a series of four over four timber sash windows (plate 44).

The ground floor level has a similar layout to that of the basement with a series of inserted rooms surrounding a larger open space, supported by evenly spaced cast iron columns orientated north to south in two rows (plates 45-46). Additional features of interest can be seen on this level with then northern extent of the room containing two segmental headed blocked up openings (plate 45). In addition to this, a lean-to structure with glass ceiling is located to the western elevation (plate 46). This features a number of plain one over one metal casement windows aligned in rows of four (plate 47). The columns to this floor are thinner and taller than the units below, but similar in their architectural detailing (plate 48). One of the original entrance doors is visible with two narrow triple panels flanked either side by vertical lights and surmounted by three segmental windows divided by timber uprights (plate 49).

The first storey is similar again, although less sub-divided. The number of cast iron columns is also reduced to one row running north-south through the centre of the room. This is due to the reduction in width caused by the absence of the lean-to structure. The fenestration pattern is similar although the western section features two over two timber sashes as opposed to the segmental ones found on the eastern façade (plate 50).

The second storey features a modern stairwell (plate 56) with modern fittings and its larger open space to the north has experienced numerous modern insertions including partition walls, suspended ceilings (plates 51-54). The fenestration pattern (plate 55) and layout of internal cast iron columns is identical to the preceding storey. The third storey again shows a similar layout to the previous storey albeit with additional modern fixtures and fittings (plates 57-58).

The attic storey follows the general layout of the preceding floors with small partition rooms located to the north and south (plates 59-60). This later date of this floor is reflected in the modern two over two casement windows located on the eastern and western elevations (plate 61).

#### 3.2.3 Analysis

Nos. 8-9 Hayne Street have been described by Pevsner as ‘an unpretentious brick warehouse c. 1870s’ (Pevsner, 2002). His description, however, makes reference to 8-10 Hayne Street. Archival research and historic map analysis show that there was in fact an additional, single-storey structure located to the north of the building (Figures 12-15). Physical evidence for this is supported by a painted scar on the ground floor external elevation of Nos. 8-9 Hayne Street. Scarring to the elevation also shows it was a two tiered structure with the western extent being taller (plate 36, figure 13). This structure was originally in use as a bacon curing structure and store (figure 12) and featured a glass roof above. Part of this building can be partially seen in figure 14 which shows a similar segmental archway rising from an impost band. A small section of this structure also survives to the northern section of Nos. 8-9 Hayne Street (plate 32). Figure 12

shows that this building was accessed via the ground floor through a double doorway located in the archway shown in figure 14. Access to this single-storey unit from Nos. 8-9 Hayne Street is via two openings as shown in figure 12, these openings were still extant at the time of survey although they had been blocked (plates 36, 45).

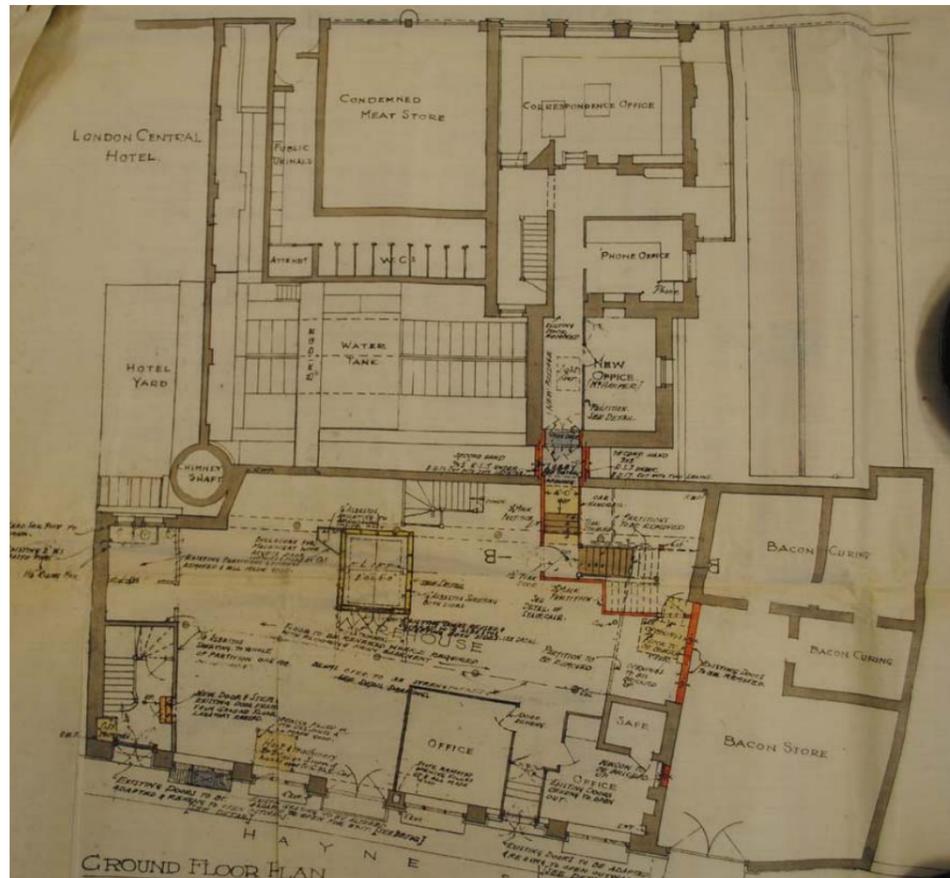


Figure 12 - 1918 ground floor plan showing 8-9 Hayne Street linked to the GWR offices to the west (Source – LMA, GLC/AR/BR/22/BA/043620)

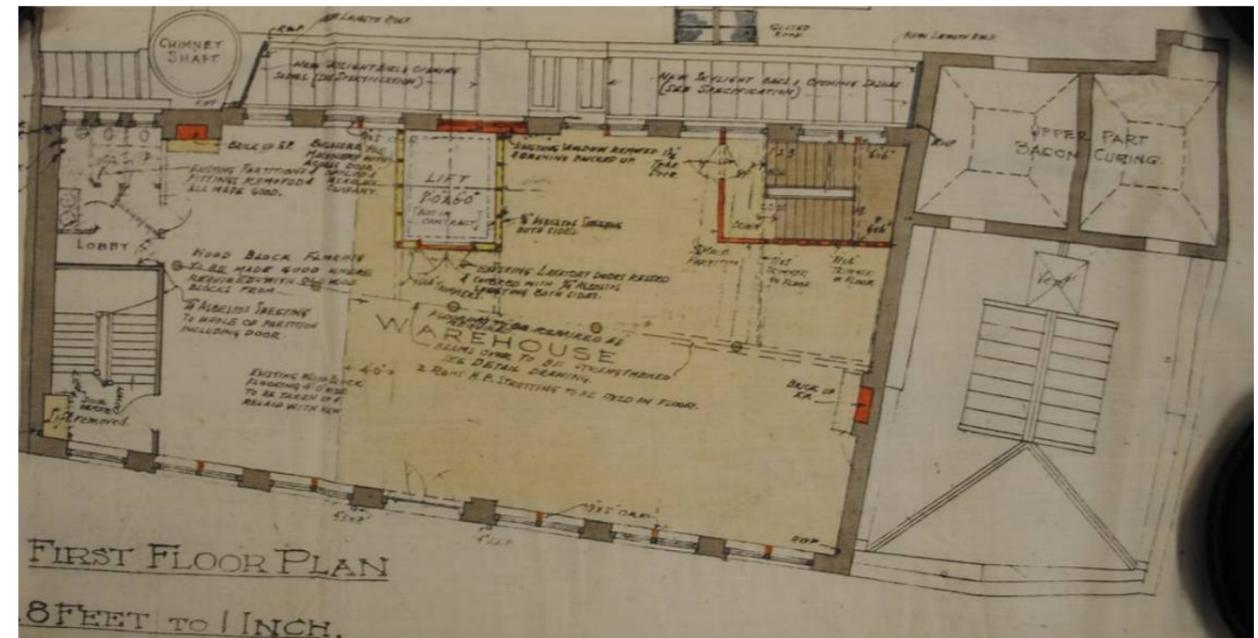
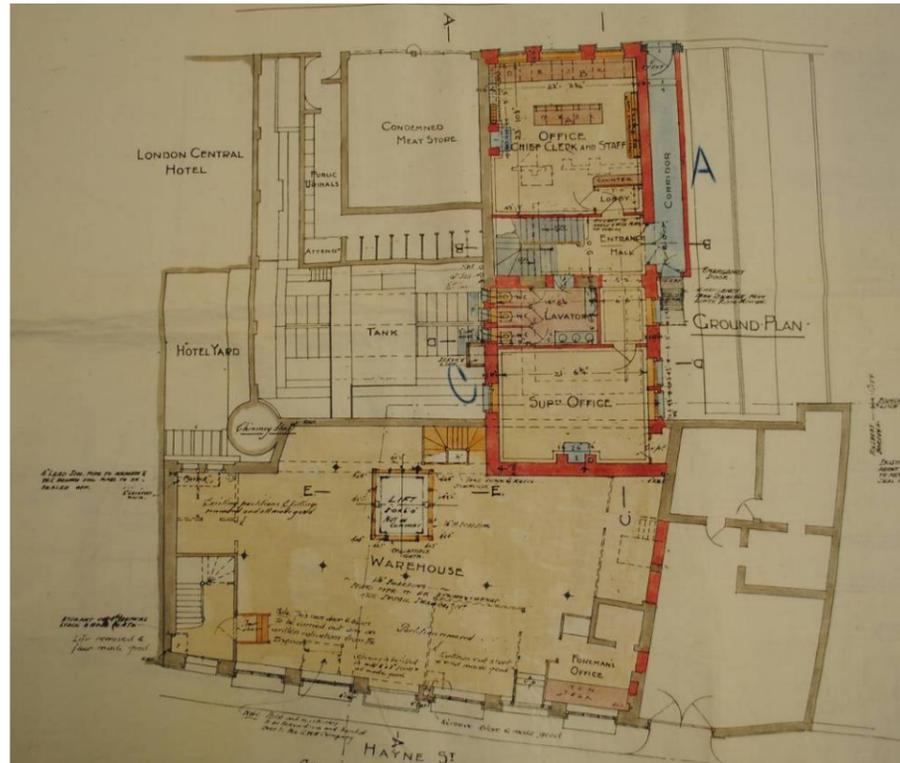


Figure 13 - 1918 plan first floor plan showing glass roof of 10 Hayne Street (Source – LMA, GLC/AR/BR/22/BA/043620)



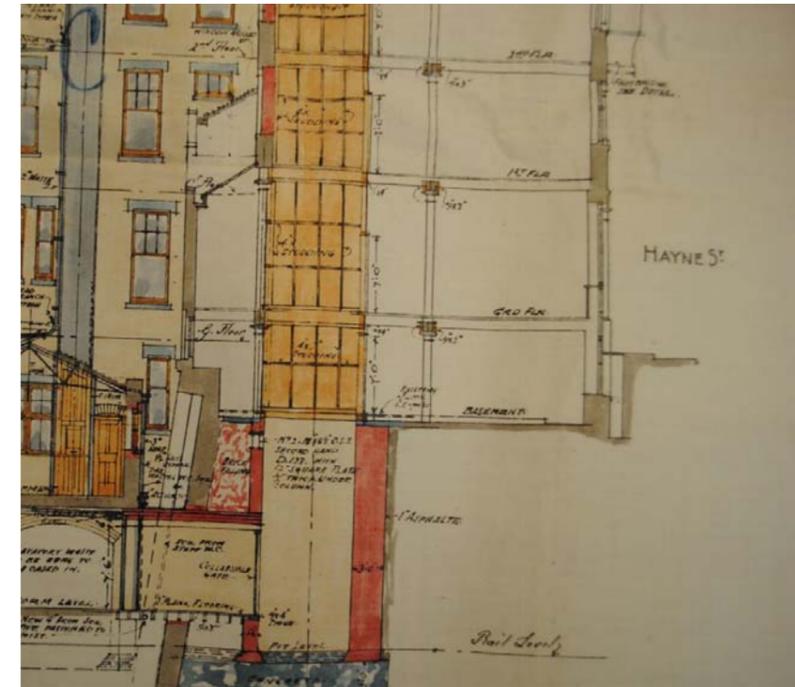
Figure 14 – 1916 plan showing original elevation of 8-10 Hayne Street (Source – LMA, GLC/AR/BR/22/BA/043620)

Historic plans show that there was a Great Western Railway (GWR) office located directly to the west of this structure, and in 1916 it was proposed to combine the two together (figure 15). These plans do not appear to have been carried out to their full extent, however, when compared to the OS maps (figure 6). Revised proposals in 1918 do seem to have been undertaken and completed, with the rear of the GWR office being linked to the ground floor of 8-9 Hayne Street via a narrow bridge (figure 12). Physical evidence for this can be clearly seen in plate 158 where the entrance doorway has been boarded up to the rear of 8-9 Hayne Street.



**Figure 15 - 8-9 Hayne Street showing 1916 plans to incorporate the GWR structure (in red) into the building (Source – LMA, GLC/AR/BR/22/BA/043620)**

One piece of work that does appear to have been carried out from the 1916 plans is the insertion of a lift shaft into Nos. 8-9 Hayne Street linking the structure via the platform level of 3 Lindsey Street (figures 16, 27). This provided direct access for goods from the sidings of Smithfield Market to 8-9 Hayne Street which shall be analysed further in the 3 Lindsey Street section.



**Figure 16 – 1916 plan showing lift shaft below 8-9 Hayne Street linking the structure to the basement of 3 Lindsey Street (Source – LMA, GLC/AR/BR/22/BA/043620)**

Trade directories show that the warehouse was used as a printers and bookbinders (Kelly's PO, 1910). It is possible that unbound papers would be transported via the railways, hoisted up into the various levels of the warehouse for binding prior to being sent out for retail. This is supported by the fact that the platform level of 3 Lindsey Street was used as a storage area during the early 20<sup>th</sup> century (figure 27).

Cartographic and plan records show evidence of their being a linking bridge between Nos. 8-9 Hayne Street and another similar warehouse opposite, originally No. 2 Hayne Street, now demolished (figures 17-19). This bridge was constructed by the Great Western Railway in 1918, with the approval of the London County Council. A condition placed upon it that it would be removed in 1927; this was later extended to 1932 when the Council's condition for its removal came into enforcement (GLC/AR/BR/22/BA/043620). The only surviving visual evidence for the bridge can be seen in the variation in the window detailing on the eastern elevation of Nos. 8-9 Hayne Street and inserted stone blocks within the masonry (plate 34). Whereas the majority of the other windows to the façade feature colonettes, the windows here which originally housed the walkway are plainer.

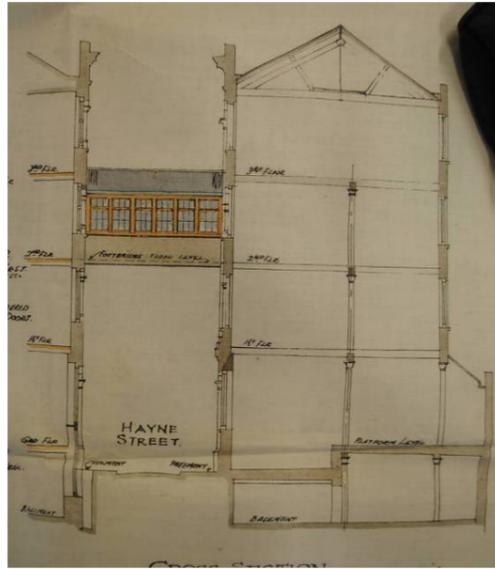


Figure 17 – 1916 elevation showing linking bridge between 8-10 Hayne Street and 2 Hayne Street (Source – LMA, GLC/AR/BR/22/BA/043620)



Figure 19 - 1916 plan showing façade of 2 Hayne Street (Source – LMA, GLC/AR/BR/22/BA/043620)

Finally, by the 1950s, records show that 8-9 Hayne had been linked to 2 Lindsey Street via a linking bridge located to the roof lofts at the top of each structure (plates 39, 64). This shall be further analysed in the following section.

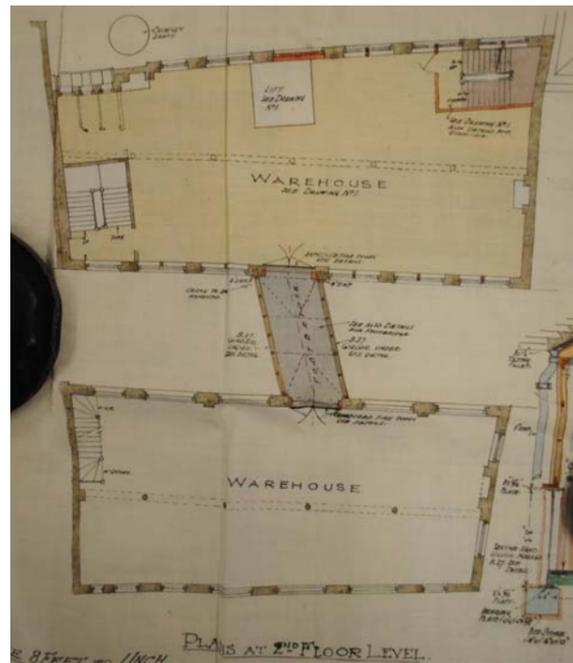


Figure 18 - 1916 plan showing linking bridge between 8-10 Hayne Street and 2 Hayne Street (Source – LMA, GLC/AR/BR/22/BA/043620)

### 3.3 Nos. 20-23 Long Lane and No. 2 Lindsey Street



**Figure 20 – 20-23 Long Lane and 2 Lindsey Street shown in green**

#### 3.3.1 External Description

Nos. 20-23 Long Lane and 2 Lindsey Street are large, four-storey structures originally built as one architectural unit dating to the late 19<sup>th</sup> century (figure 20). Later alterations were undertaken in 1940 to 22-23 Long Lane and 2 Lindsey Street remodelling the buildings in the Art Deco and International style and applying an additional attic storey (Builder, 1949).

To the Long Lane elevation, No. 23 Long Lane is four bays in width featuring a large, slightly off centre doorway flanked either side by five over five vertically aligned window openings, the eastern unit being two panes in width and the western, five. Above this is located a plain frieze with applied Art Deco signage surmounted by a projecting sill course. The fenestration pattern above features three storeys of Crittall style windows. Emphasis is afforded to the first floor with larger windows. Dividing the fourth floor from the attic storey is another, smaller frieze with projecting sill band above and below, enclosing a modern painted running ornament. The attic storey incorporates a chimney to the east and two horizontally orientated International Style windows comprising nine panes, the upper most central one being of a casement construction. A heavily projecting flat roof is located directly above this (plate 62).

The Lindsey Street elevation is similar in its architectural detailing but longer, being nine bays in length. Three large openings, identical to the large unit located on Long Lane, are located to the ground floor with a modern entrance doorway located to the north. The attic storey is similar again to the southern elevation, but features four windows.

The northern elevation provides an insight into the appearance of a section of the structure prior to its remodelling of the façade in 1940. This elevation is very plain in its appearance, lacking any architectural ornament. The ground floor features no windows, whilst the main emphasis can be seen to the centre of the structure with two, one over one timber sash windows located on the first to third storeys (plate 84). A number of modern small windows are located to the east providing light for the toilet facilities and to the first floor are two Crittall type windows at slightly different levels (plate 64).

The rear façade, to the east, is doglegged in appearance and slightly concealed by an external steel fire escape. The windows are a mix of coupled two over two timber sashes with segmental heads, and tripartite timber sashes with flat heads and with a two over two form (plates 86). A blocked doorway was discerned at the summit of the eastern dogleg (plate 64).

Moving back to Long Lane, number 22 is identical to its western neighbour, but lacks the attic storey. The ground floor has been heavily modernised with a large glass entrance way. No. 20 Long Lane is four storeys in height with red brick and painted stone dressings. The ground floor has been completely modernised with blue bricks and vertical concrete shuttering, removing any historic fabric (plate 67). The elevation to Long Lane is four bays in width with slightly projecting sill bands dividing each level. A shallow parapet is located to the roof surmounted on a moulded cornice. The windows have been modernised with reflective glass and metal surrounds and feature centrally upper hinged casements surmounted by a horizontal stone lintel. A canted corner encloses single windows to the first to third floor, surrounded by stone quoins (plate 68). To the Hayne Street elevation is a similar pattern to that of Long Lane, although reduced to three bays in width with regular large spaces between each aperture (plate 68).

#### 3.3.2 Internal Description

The basement area of the structure was largely inaccessible at the time of survey, however, post demolition, a number of brick vaults to the south were uncovered as well as a number of splayed tiled basement lights to west (plates 69-71). The eastern vault is located under what would have been the canted section of 20 Long Lane, and as such it is at a 45 degree angle to Long Lane. The vault is a relatively shallow unit consisting of stock bricks laid in the English Bond (plate 72). The floor is a mixture of rubble and modern flooring tiles. The front of the structure had been partially faced by thick stock brick walls forming part of the basement walls of the structure above. The next series of vaults located to the west are two vaults joined together by a thin brick wall in which is located a blocked up small round headed opening (plate 73). These vaults are similar in their composition featuring stock bricks with an uneven segmental headed ceiling and slightly curved wall to the rear (plates 74-75).

Further west are another pair of similar vaults, this time their central low access opening remains unblocked (plates 76-77). Another four vaults of similar dimensions are located to the west of this (plate 78). One of these has been completely bricked up (plate 79), two internally modified creating smaller spaces (plates 79-80) and another encased in concrete (plate 81). The final two vaults were wider than the previous ones and have lower masonry walls creating a very shallow arched ceiling above (plates 82-83). Much of the interior has been treated with concrete although a stock brick construction was visible in some places.

No vaults were visible to the west of the structures along Lindsey Street, although a series of late 19<sup>th</sup> century splayed windows with white glazed brickwork was visible along this elevation providing light from the footpath above (plate 84). Prior to the demolition of the structures an internal survey of the building was undertaken and any items of interest were documented as follows.

The interior of No. 20 Long Lane has been completely modernised (plate 85) and as such shall not be discussed in detail. The main interest of the structure is in its external envelope and the comparisons to be made between it and its modified neighbour to the west. The basement area of Nos. 22-23 Long Lane and No. 2 Lindsey Street have been heavily modernised and retains little of either its original composition or the 1940 modifications. Modern stairwells and inserted ceilings and plaster boarded walls (plates 86-87) are predominant throughout, even the central areas which house the heating units, have little of historic interest with only some modern brick partition walls being evident (plates 88-89).

Moving to ground-floor level, No. 22 Lindsey Street is narrow and long in its composition and features light timber floor boards and a plasterboard ceiling with partially visible I-beams (plates 90-91). To the north of the room is a roof light of six over six metal framed lights with four panes acting as a casement permitting light into the space (plate 92). The interior of the ground floor of No. 23 Lindsey Street contains a number of modern features, including suspended ceilings, carpets and internal partition walls (plates 93-96). To the north, in No. 2 Lindsey Street is the original entrance to the structure, now primarily used for access to the basements and upper storeys. Modern fixtures and fittings were dominant here with little of historic interest surviving (plates 97-98).

Access to the upper storey was provided by a carpeted stairwell, with projecting plastered dado located on either side (plate 99). The landing to the upper storey features plain balusters and handrail to the stairwell along with a lift in its 1940 location (plates 100-101). Moving to the south, the main body of the building opens up featuring a suspended ceiling and vertical stanchions enclosed by modern plasterwork (plate 102). Direct access to No. 22 Lindsey Street is possible by three door openings with modern moulded door surrounds (plate 103). Upon entering No. 22 Lindsey Street, the partition wall between the two properties is noticeable by the depth of the wall (plate 104). This floor is similar to the ground floor although to its northern extent are located three, one over one timber sashes (plate 104). Moving to the second and third floors, the structure of the rooms is similar to that which preceded it (plates 105-106), although the outline of the stanchions to the third storey are more discernible, with an outline of their capitals being visible, alongside the girders above (plate 106). Slight variation was discerned on the third floor level of No. 22 Lindsey Street with the insertion of two modern doors to the southern extent of the building (plate 107).

The attic storey of No. 23 Lindsey Street and No. 2 Lindsey Street was similar again to the rooms below but feature an original 1940 sturdy four horizontal panelled door providing access to the lift shaft (plate 109). Access to the roof space is provided by a series of timber steps leading to a panelled door with two horizontal apertures above (plate 108).

Above this is located a flat roof structure with two small square structures to the north. The smallest of these features an eight paned Crittall style window (plate 110). The largest features two louvres to its southern and eastern extent and a small projecting louvre above. The western extent also featured a loading door with hoist beam above (plate 111).

### 3.3.3 Analysis

Historic map regression shows that the structure was built in the late 19<sup>th</sup> century. This is supported by the fact that a number of sash pulleys located in the rear of 2 Lindsey Street were made by Archibald Kenrick & Sons Ltd, who supplied manufacturing goods from the black country (C. Brooking, pers. comm. 24/06/10). The structure's original appearance prior to its conversion can be seen in figure 21.

The vaults exposed to the south of the site are also likely to be contemporary with the buildings, being constructed of the same brick work and with similar construction techniques. Figures 3 and 4 show that prior to the construction of Nos. 20-23 Long Street and No. 2 Lindsey Street there were structures located directly above these vaults, discounting them as being any earlier.



Figure 21 – Structure prior to its conversion (source – The Builder, 1945)

Historic trade directories show that the structures originally housed numerous businesses, with No. 21 Long Lane being in use as a butchers, 22 Long Lane, a gold blockers and 23 Long Lane a temperance hotel (1895 London Directories). The temperance movement was at its height at this point, extolling the virtues of abstinence. The GWR's influence over the area had spread by 1938, with records showing that 22 Long Lane was now used as a booking and receiving office by the GWR. In addition, the temperance hotel had become a meat importers run by Armour and Co. Ltd (Kelly's P.O. 1938) whilst 18-21 Long Lane was in use as cold storage company. It was Armour and Co. Ltd that initiated the remodelling of the structure in 1940, although they initially appear to have used the structure as they had found it as displayed in figure 22.



Figure 22 – 23 Long Lane prior to conversion (source, NMR, bl 28770 7)

The GWR's influence in the area by the mid-1950s seemed to have dwindled, reflected in the fact that No. 22 Long Lane had changed uses again, now accommodating a wholesale glove maker and hosiery merchants (Kelly's P.O. 1953). Armour and Co.'s operations in the 1950s appear to have branched out with the construction of a bridge linking the top storey of No. 2 Lindsey Street with the top of Nos. 8-9 Hayne Street (figure 23). This explains the two blocked doors located to both structures (plates 39, 64). It also explains the existence of a hoist beam at the top storey. Presumably, meat would be delivered via the railways, loaded into the lift shaft via the basement of 3 Lindsey Street, raised to the top storey of 8-9 Hayne Street via the lift shaft and moved across to 2 Lindsey Street via the newly constructed bridge.

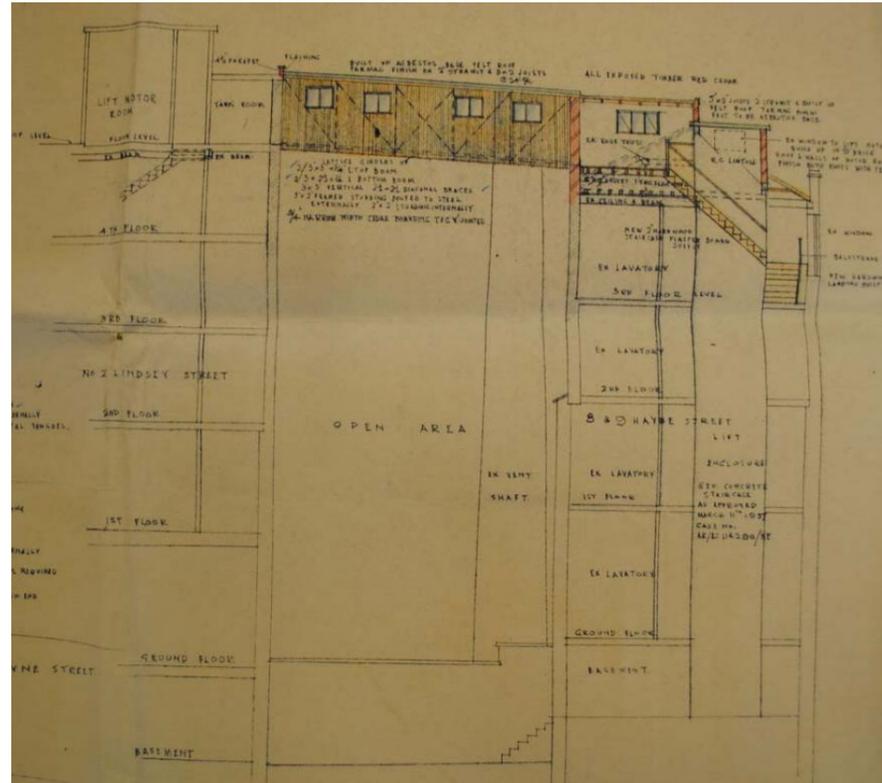


Figure 23 – 1950s drawing showing linking bridge between 8-9 Hayne Street and 2 Lindsey Street. Also note height of chimney to rear (Source – LMA, GLC/AR/BR/22/BA/043620)

### 3.4 No. 3 Lindsey Street



Figure 24 – 3 Lindsey Street shown in green

#### 3.4.1 External description

3 Lindsey Street is a single storey, late 19<sup>th</sup> century structure surmounted by a timber louvre with a series of shallow hipped roofs (figure 24). Its principal elevation, located to the west, fronts onto Lindsey Street. The building is flanked on its south and eastern elevation by a toilet block (plate 113). The façade is largely made up of buff coloured ceramic tiles located above the entrance, some of which incorporate parts of blue lettering spelling out the former owners name and function (plates 112 and 115). The entranceway incorporates two large corrugated iron roller doors, the southern one of which features a small door. The northern section features a decorative fluted stone pilaster with crocket capital, plain frieze and moulded architrave above (plate 114). This feature is part of a surviving wall that separated No. 3 Lindsey Street with the Great Western Railway correspondence office to the north figure 12.

#### 3.4.2 Internal description

The internal arrangement comprises four levels, one platform, one basement, one ground floor and one mezzanine level. The platform and basement levels of the structure although located below, are not in fact associated with the building and were only accessible via the courtyard to the rear. Entrance into the structure was facilitated by a large archway which had previously been blocked up, revealed during the demolition work (plate 116). The platform level comprises three rectangular rooms orientated east to west. Access to the southern room was difficult due to dense backfill rubble, however, a blocked up aperture to the south of the room is visible with steel lintel above. This revealed a concrete rendered jack arched ceiling lined with small I-beam girder (plate 117). In the northwest of the room is a small opening with round head

(plate 118, 123). In the central room, the formerly blocked up entrance arch is visible to the east (plate 119) and two smaller doorways with segmental heads located to the north and south (plate 120 and plate 122). Evidence of a former rolling door was discernible above this doorway through the existence of a bracket above (plate 122). Also on this elevation is the other side of the small aperture, now blocked with timber boards (plate 123). Another slimmer door to the north provides access to an additional room. The ceiling here is also of a jack arch construction (plate 121). The western end of the room is also slightly raised (plates 124-125).

The last room to the north is open over two storeys. On its southern façade, at basement level, are located three blocked windows with projecting sills (plate 127). Access to the upper storey is provided by a modern steel staircase, installed as part of the demolition works (plate 128). Two bricked up doorways are visible at the platform and track level on the eastern side of the room (plate 129). After demolition, the northern extent of this room was more easily discerned, showing a large bricked up archway to the centre, flanked to the east by a smaller arch, above which is the top of a large archway, also bricked up (plate 130). To the west of this is a tall narrow archway with a floor located near its summit (plate 132). Another interesting feature in this room is a rectangular hole located within the retaining wall, linking the room via a flue to a chimney to the south (plate 131). On the upper section of this room, within the retaining wall to the west, are located two inverted railway tracks (plate 133).

To the south of these tracks, at basement level are located a further two rectangular rooms. The northern section is flanked to the west by a modern brick wall with five window openings, behind which is located a retaining wall (plate 134). Towards the south of the room are two blocked segmental headed windows flanking a blocked central doorway (plate 135). The southern room had a new structure inserted into it, with part of the eastern retaining wall still being visible (plate 136).

At ground floor level, to the south of the main structure is the 'L' plan toilet block. The southern exterior contains a number of modern urinals with cisterns above (plate 137). Moving to the east, a number of modern partitioned toilet blocks were visible (plate 138). Light is provided from the roof by glass and metal sloped roof.

Within the main body of the building there is very little of historic interest with most of its original features being removed or obscured by modern plaster (plates 139-140). Access to the mezzanine level is provided by a modern narrow metal stairwell to the southeast (plate 141). Evidence of an earlier shop front with large sheets of plate glass and hinged window above were visible behind the southern roller door. Although the glass work is likely to be later, the moulded uprights with fluted sides are contemporary with the original construction (plate 142).

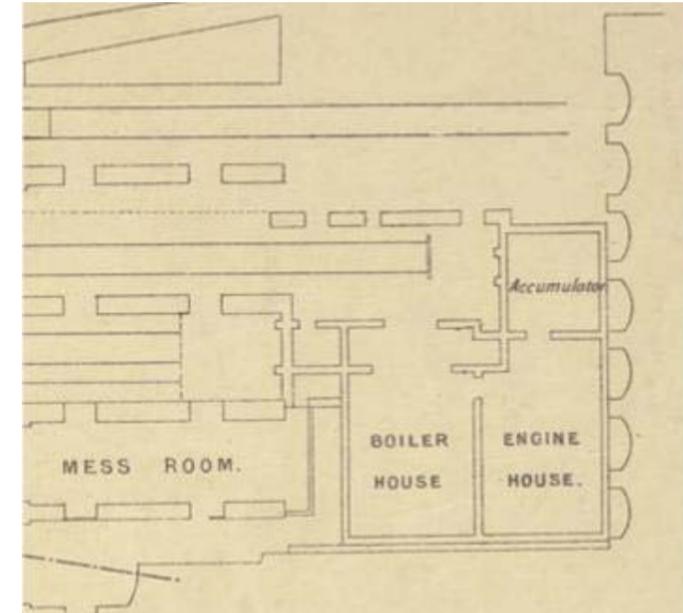
The mezzanine floor reveals two sets of three lights located in the apex of the roof just below the external timber louver, providing light to a lobby area (plate (143 and 145). The roof structure was exposed and presented four fink trusses located at right angles together supported on a steel framework below (plates 143-145). A metal ladder was located within the roof space providing access to the timber louver above (plate 146). The partitioned room to the north of the structure contains a wash room area including Belfast sink (plate 147).

### 3.4.3 Analysis

No. 3 Lindsey Street dates to the late 19th century and first appears on the 1896 OS map. It is likely that the structure was always in use as a meat store, as shown in figure 15, although there is no mention of Edmund Martin Ltd in trade directories at this time, suggesting that the ceramic fascia is of a later date. The external façade is of interest as it features the sole remnant of the (now demolished) Great Western Railway correspondence office in the form of a stone pilaster (plate 114). Historic plans show that the structure to the south of the meat store was historically used as a public lavatory, with the cubicle in the far end being used as an attendant's booth (figure 12).

Historic map analysis has shown that the basement level and platform level below were not historically part of 3 Lindsey Street. This is supported by the on-site analysis which revealed no existing or previous access

routes between the levels. The structures appear to have had links with Smithfield Market historically with plan drawings dated to 1931, but based on earlier maps, showing the platform rooms in use as an engine room and boiler room (figure 25). This would explain the need for a flue to the base of the retaining wall and also the existence of the chimney.



**Figure 25 - 1931, GWR Smithfield plan based on earlier map (Source – LMA, GLC/AR/BR/22/BA/043620)**

By 1916, however, these rooms appear to be linked with the GWR correspondence office located at No. 4 Lindsey Street (figure 26). The structures to the platform level were used as storage rooms for goods going into Nos. 8-9 Hayne Street via the lift shaft. These goods would have been delivered via the railway sidings to the north. As already mentioned, it is likely by the 1950s that animal carcasses were being transported here and transferred to No. 2 Lindsey Street via Nos. 8-9 Hayne Street. The basement storey above housed a mixture of toilets, stores, kitchen and a men's mess room that were directly linked to No. 4 Lindsey Street (figure 26).

Blocked openings located within the basement area did not correspond with any historic plans although it is more than likely that these windows and doors would at one point have linked the structure to the GWR offices (plate 135). A number of other openings do correspond with those identified in historic plans, supporting the idea that the structure was linked to the GWR offices (plates 129). In addition to this a number of the larger arched openings would have originally provided access for goods coming in from the railway sidings (plates 116, 119, 126). The entranceway to the lift shaft that linked the platform levels to 8-9 Hayne Street was also discerned being located within the eastern retaining wall (plates 117, 155, 160).

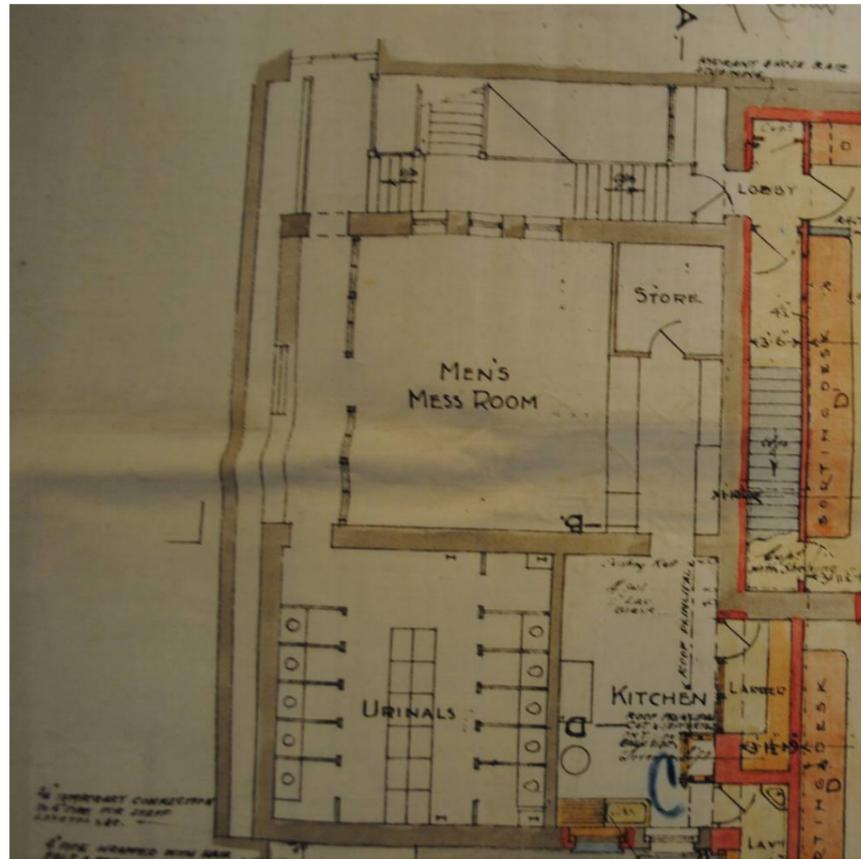


Figure 26 – Basement plan of 3 Lindsey Street showing direct access to the GWR offices to the right (Source – LMA, GLC/AR/BR/22/BA/043620)

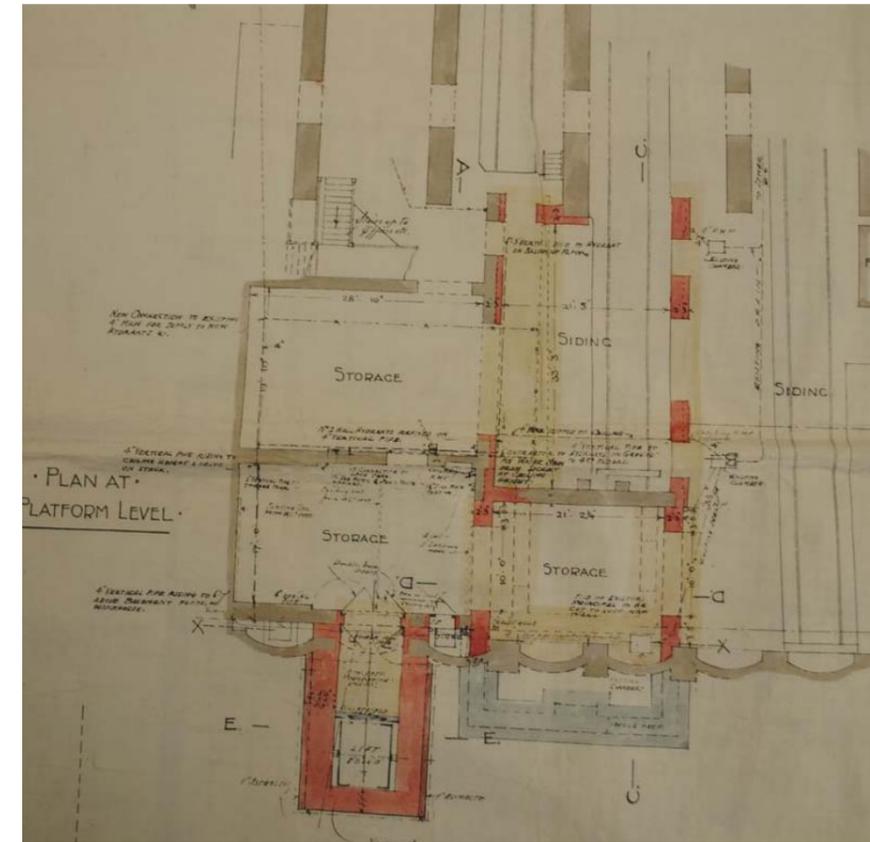


Figure 27 – Platform plan of 3 Lindsey Street showing lift shaft to bottom linking 8-9 Hayne Street to the structure (Source – LMA, GLC/AR/BR/22/BA/043620)

### 3.5 Chimney to the rear of No. 20 Long Lane and the retaining arches below Nos. 8-9 Hayne Street and No. 23 Long Lane



**Figure 28 – Chimney and retaining walls shown in green**

#### 3.5.1 Description

The chimney and retaining walls were built in the late 19<sup>th</sup> century and are located towards the centre of the site. They consist of a south and east wall which act as a support to Nos. 20-23 Long Lane, No. 2 Lindsey Street and Nos. 8-9 Hayne Street. The brick chimney is located at the intersection of these two retaining walls as shown in figure 28. The chimney consists of heavily soiled stock bricks laid in an English Bond (plate 148). The base of the structure, at ground level, features a slightly projecting plinth made of similarly laid brick courses (plate 149). The top of the chimney is treated with a layer of mortar, suggesting that the structure has been truncated at some point (plate 150).

The base of the chimney is accessible via a later inserted door in the basement level of No. 3 Hayne Street, which is reflected in the modern Fletton brick work and concrete supporting lintels (plates 151-152). It is presumed that this chimney contains a further flue that runs behind the southern retaining wall to the opening located at platform level of No. 3 Lindsey Street (plate 153).

The southern retaining wall is a two tiered structure with a ledge recessing backwards approximately half way up its height. The structure is made from stock bricks (now heavily soiled) and projecting string course near its summit (plates 153-154).

The chimney and southern wall are connected to another retaining wall to the east. This retaining wall contains seven shallow arches with segmental heads. The second niche from the south contains a square blocked up space constructed of brick with steel lintel above that was formerly located in the basement of 3 Lindsey Street (plates 154, 155, 160). To the north of the wall is located a large brick masonry wall which acts as a terminus (plate 156). Located above the arches is a three course projecting brick sill band. The structure previously supported 8-9 Hayne Street above (plate 159)

#### 3.5.2 Analysis

The retaining arches date to the mid-19<sup>th</sup> century and truncated brick chimney to the late 19<sup>th</sup> century (see figure 4) and are significant surviving elements of the development of the Eastern Ticket Hall site. The chimney is significant for its potential ties with the function of Smithfield's Market, whilst the row of brick retaining arches are of interest for their structural contribution to the development of Hayne Street.

The structure is clearly visible on the 1873 OS map (figure 4), and as such are the oldest structures within the site. It would appear that the chimney was originally used in conjunction with the boiler and engine rooms at the platform level of Smithfield Market (figure 25). Although its use by the early to mid-20<sup>th</sup> century seems to provide ventilation for the operations of the GWR at that time (figure 23). It is clear from this figure that the chimney was originally much taller in height, supporting the idea that it was truncated at a later date.

## 4 Conclusion

The site presents a complex, but interesting evolution of historic structures whose form and function are determined by the logistical constraints that accompany building in such a confined and complicated space. The study shows that the earliest structures on the site are the retaining walls, constructed as part of the improvement works of the mid- to late 19<sup>th</sup> century. Many of the buildings have lost much of their internal historic fabric, although a number of pilasters, shop front mouldings, windows and interesting construction techniques were still visible, providing historic and architectural interest. The main interest of the buildings lies in their changing interrelationships with one another discerned through both historic archival data and the existing fabric of the buildings.

The survey shows that Nos. 8-9 Hayne Street was a central structure within the area having links to virtually all the buildings on the site. The structure was originally used as a book binders before being partially purchased by the GWR in the early 20<sup>th</sup> century and linked to their offices at No. 4 Lindsey Street via a ground-floor bridge. In addition to this, the structures were also linked via a lift shaft that penetrated the eastern retaining wall via the platform area of No. 3 Lindsey Street. This platform area was originally part of the GWR offices and linked to a number of railway sidings that provided goods to the offices via Smithfield's Market. Evidence also suggests that this area may have originally been part of the market building, being used as an engine and boiler room, explaining the need for the chimney.

By the 1950s, the GWR appear to have been in a steady decline and it is at this point that the owners of Nos. 22-23 Long Lane and No. 2 Lindsey Street acquired access to Nos. 8-9 Hayne Street via a bridge linking the upper storeys of the buildings. This was presumably used to bring fresh meat up from the railway tracks into the structures at Long Lane.

The links between the structures do not end there. To the north, Nos. 54-64 Charterhouse Street is an interesting concrete and steel structure supported on a vast steel raft located above the rail tracks below. The structure was originally linked to No. 8-10 Hayne Street (prior to No. 10's demolition) via a crane gantry. Nos. 8-9 Hayne Street was also at one point linked to an additional GWR office to the east via a bridge constructed in 1916 providing further warehouse space for the company.

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- 1914, Ordnance Survey map. Scale 25": 1 mile
- 1938, Ordnance Survey map. Scale 25": 1 mile
- 1953, Ordnance Survey map. Scale 25": 1 mile
- 1961, Ordnance Survey map. Scale 25": 1 mile
- 1970, Ordnance Survey map. Scale 25": 1 mile
- 1983, Ordnance Survey map. Scale 25": 1 mile
- 1988, Ordnance Survey map. Scale 25": 1 mile

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### Other

Charles Brooking was present during a site visit on 24/06/10 and was invaluable for his insights into the dating of sash pulleys.

## Appendix 1 - Photo location plans

### 54-64 Charterhouse Street



Plate 1 - 54-64 Charterhouse Street, north and west elevations



Plate 2 - 54-64 Charterhouse Street, northern elevation



Plate 3 - 54-64 Charterhouse Street, single storey former workshops to northern elevation



Plate 4 - 54-64 Charterhouse Street, western elevation



Plate 6 - 54-64 Charterhouse Street, window detail to first floor above carriage arch



Plate 5 - 54-64 Charterhouse Street, window detail to first floor



Plate 7 - 54-64 Charterhouse Street, rear elevation



Plate 8 - 54-64 Charterhouse Street, rear elevation



Plate 10 - 54-64 Charterhouse Street, rear elevation of former workshops



Plate 9 - 54-64 Charterhouse Street, rear elevation of former workshops



Plate 11 - 54-64 Charterhouse Street, ground floor detail



Plate 12 - 54-64 Charterhouse Street, ground floor detail



Plate 14 - 54-64 Charterhouse Street, former loading doors, one partially boarded



Plate 13 - 54-64 Charterhouse Street, internal door to Lindsey Street



Plate 15 - 54-64 Charterhouse Street, ceiling girder detail



Plate 16 - 54-64 Charterhouse Street, moulded picture rail to former office

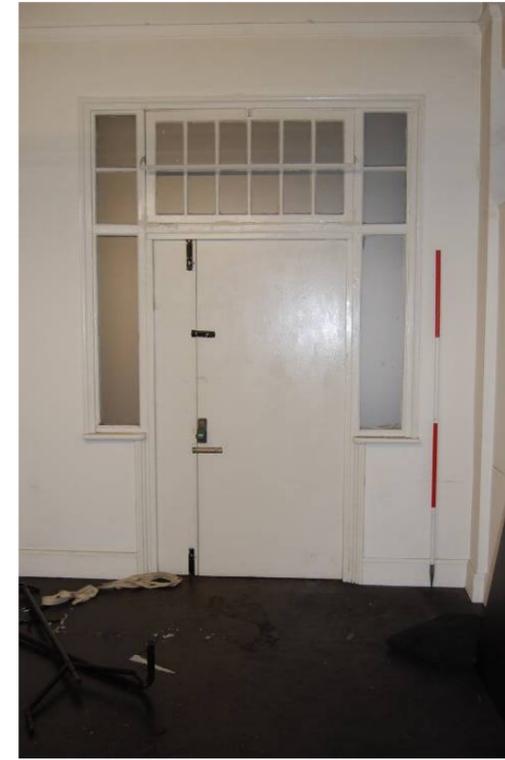


Plate 18 - 54-64 Charterhouse Street, office door detail



Plate 17 - 54-64 Charterhouse Street, ground floor office detail



Plate 19 - 54-64 Charterhouse Street, office window detail



Plate 20 - 54-64 Charterhouse Street, office detail



Plate 22 - 54-64 Charterhouse Street, concrete stair detail

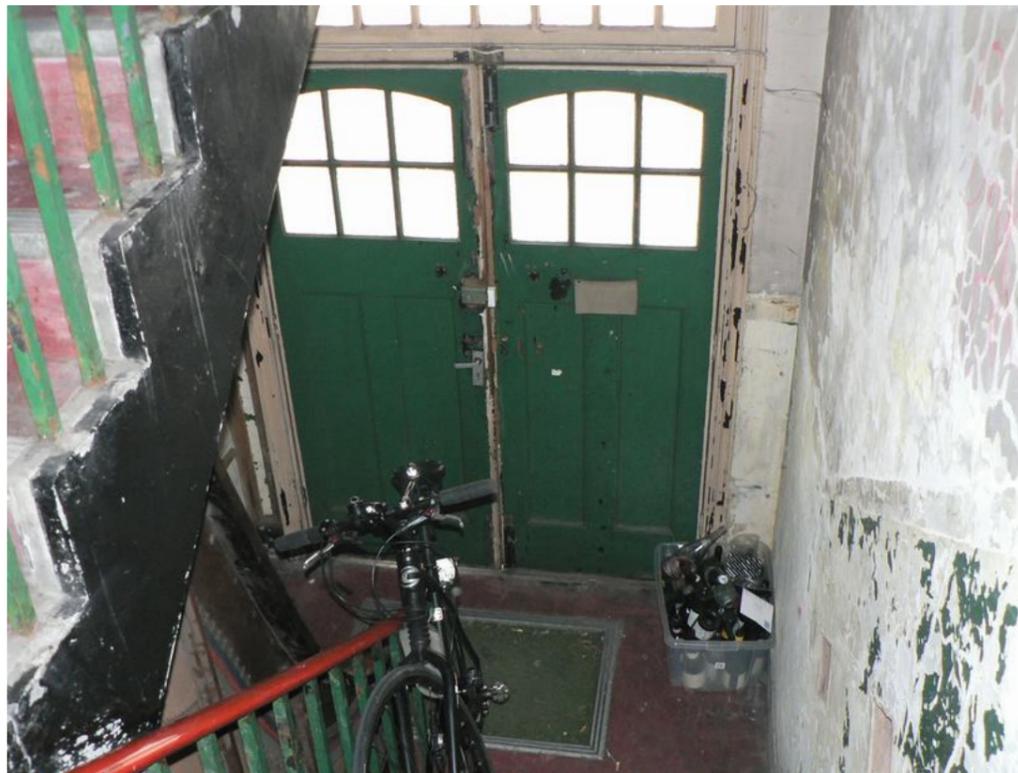


Plate 21 - 54-64 Charterhouse Street, entrance door detail

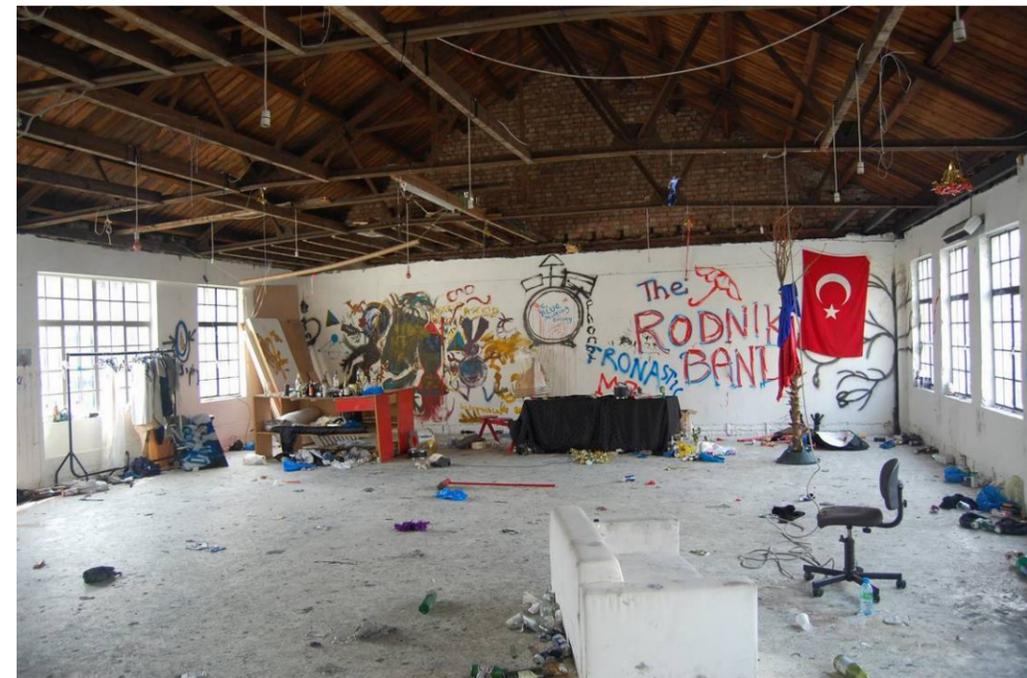


Plate 23 - 54-64 Charterhouse Street, first floor detail

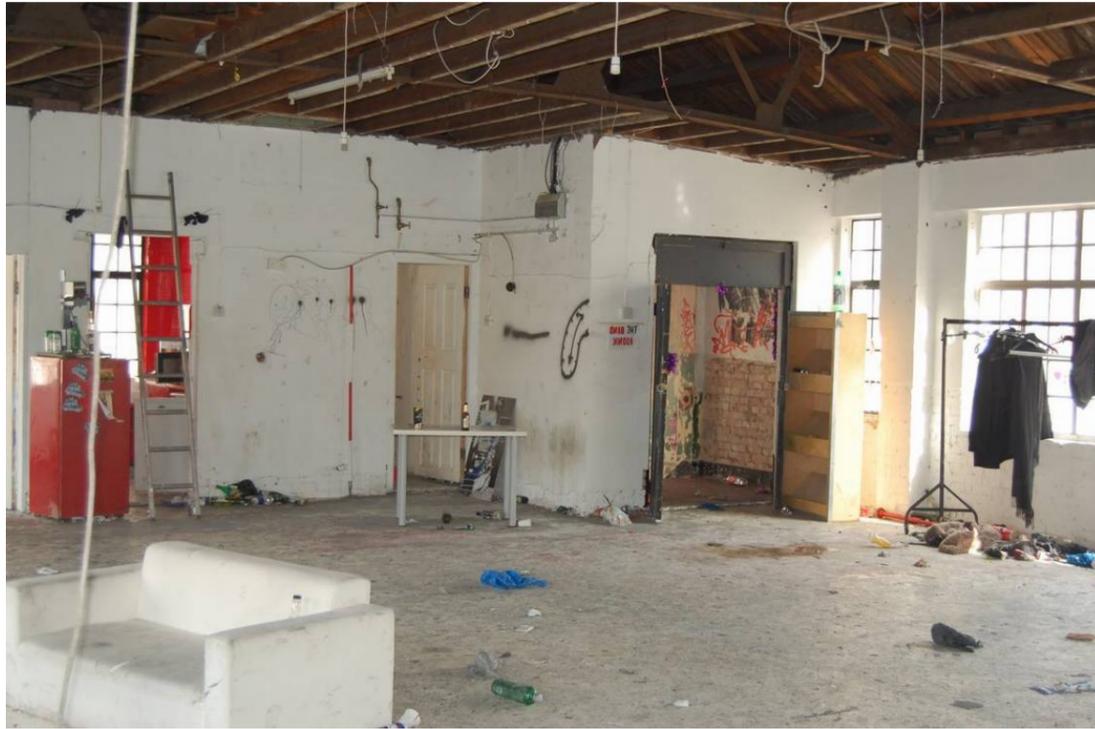


Plate 24 - 54-64 Charterhouse Street, first floor detail



Plate 26 - 54-64 Charterhouse Street, first storey detail



Plate 25 - 54-64 Charterhouse Street, double 'W' truss

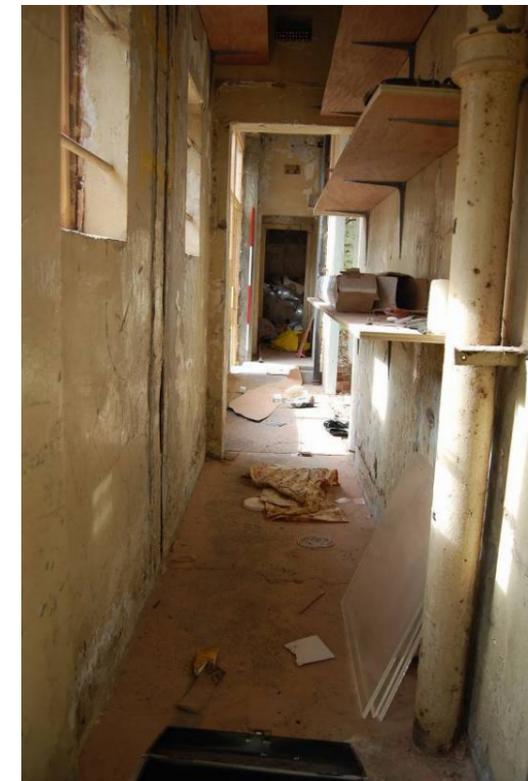


Plate 27 - 54-64 Charterhouse Street, corridor to single storey former workshops



Plate 28 - 54-64 Charterhouse Street, single storey former workshop detail



Plate 30 - 54-64 Charterhouse Street, post demolition showing decking structure upon which the structure sat



Plate 29 - 54-64 Charterhouse Street, single storey former workshop detail

8-9 Hayne Street



Plate 31 – 8-9 Hayne Street, facade



Plate 32 - 8-9 Hayne Street, facade



Plate 33 - 8-9 Hayne Street, coupled window detail



Plate 34 - 8-9 Hayne Street, inserted window after removal of walkway. Note stones either side of lintel



Plate 35 - 8-9 Hayne Street, northern elevation, note modern attic storey visible through modern brick



Plate 36 - 8-9 Hayne Street, showing blocked up apertures that used to lead into 10 Hayne Street, note also scarring to right of photo showing former roof over railway sidings



Plate 37 - 8-9 Hayne Street, showing boundary plaques



Plate 38 - 8-9 Hayne Street, rear elevation



Plate 39 - 8-9 Hayne Street, view to rear, blocked up door that previously housed a bridge to 2 Lindsey Street



Plate 40 - 8-9 Hayne Street, external view of toilet block



Plate 41 - 8-9 Hayne Street, general view

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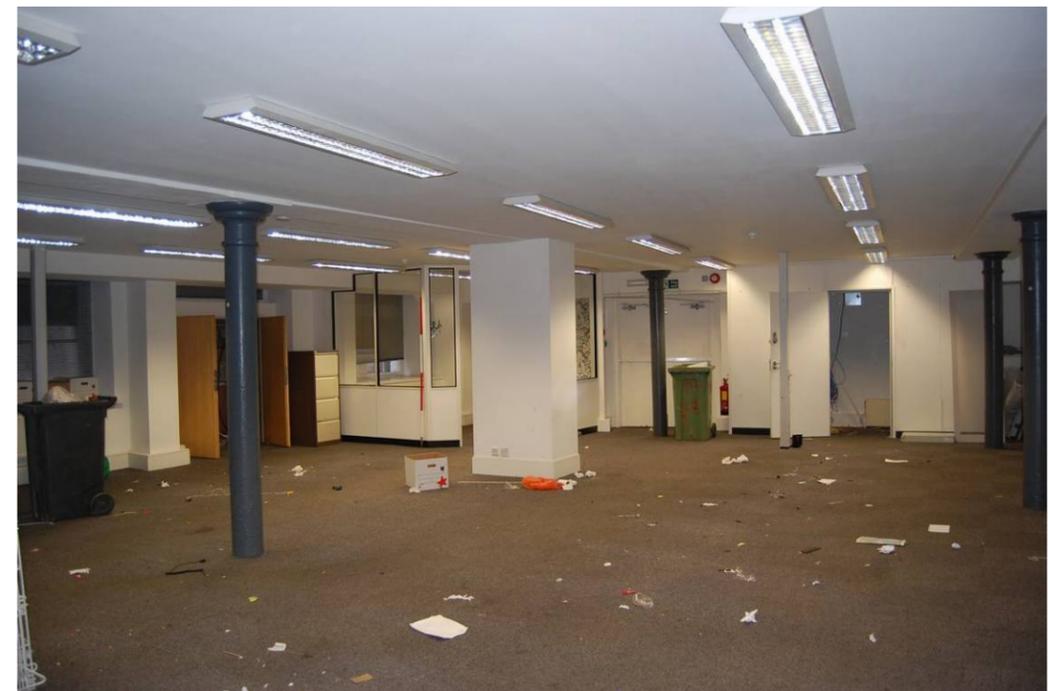


Plate 42 - 8-9 Hayne Street, general view



Plate 43 - 8-9 Hayne Street, cast iron column detail



Plate 44 - 8-9 Hayne Street, window detail



Plate 45 - 8-9 Hayne Street, general view showing blocked archways in distance



Plate 47 - 8-9 Hayne Street, window detail



Plate 46 - 8-9 Hayne Street, general view



Plate 48 - 8-9 Hayne Street, cast iron column detail



Plate 49 - 8-9 Hayne Street, entrance way detail



Plate 51 - 8-9 Hayne Street, general view



Plate 50 - 8-9 Hayne Street, general view



Plate 52 - 8-9 Hayne Street, general view



Plate 53 - 8-9 Hayne Street, cast iron column detail



Plate 55 - 8-9 Hayne Street, window detail



Plate 54 - 8-9 Hayne Street, general view



Plate 56 - 8-9 Hayne Street, landing detail



Plate 57 - 8-9 Hayne Street, general view



Plate 59 - 8-9 Hayne Street, general view



Plate 58 - 8-9 Hayne Street, window detail



Plate 60 - 8-9 Hayne Street, general view



Plate 61 – 8-9 Hayne Street, modern window detail to attic storey

20-23 Long Lane and 2 Lindsey Street



Plate 62 - 20-23 Long Lane and 22 Lindsey Street, showing 23 Lindsey Street



Plate 63 - 20-23 Long Lane and 22 Lindsey Street showing Lindsey Street elevation



Plate 64 - 20-23 Long Lane and 22 Lindsey Street, showing northern elevation of 2 Lindsey Street, note blocked door to top of eastern façade that once linked the structure to 8-9 Hayne Street



Plate 65 - 20-23 Long Lane and 22 Lindsey Street, showing rear of 23 Long Lane and 2 Lindsey Street. Note chimney to left of picture



Plate 66 - 20-23 Long Lane and 22 Lindsey Street, façade of 22 Long Lane

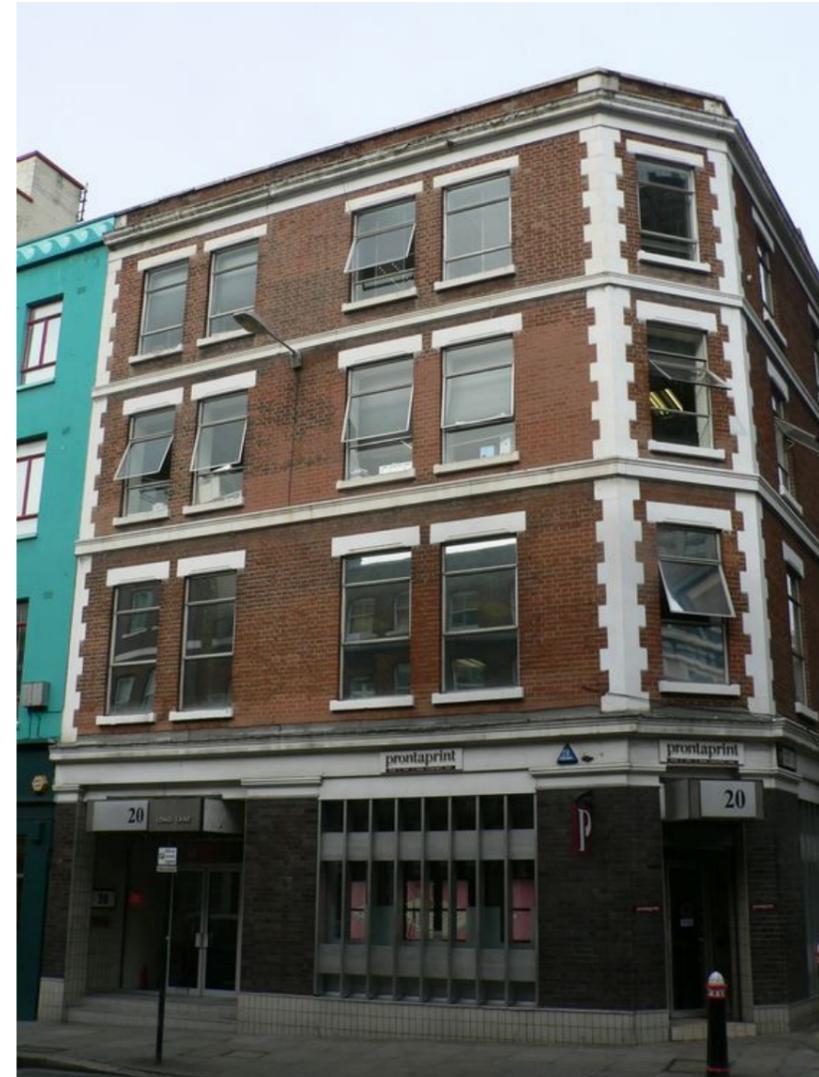


Plate 67 - 20-23 Long Lane and 22 Lindsey Street, façade of 20 Long Lane



Plate 68 - 20-23 Long Lane and 22 Lindsey Street, corner of 20 Long Lane



Plate 70 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 69 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 71 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 72 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 74 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 73 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition

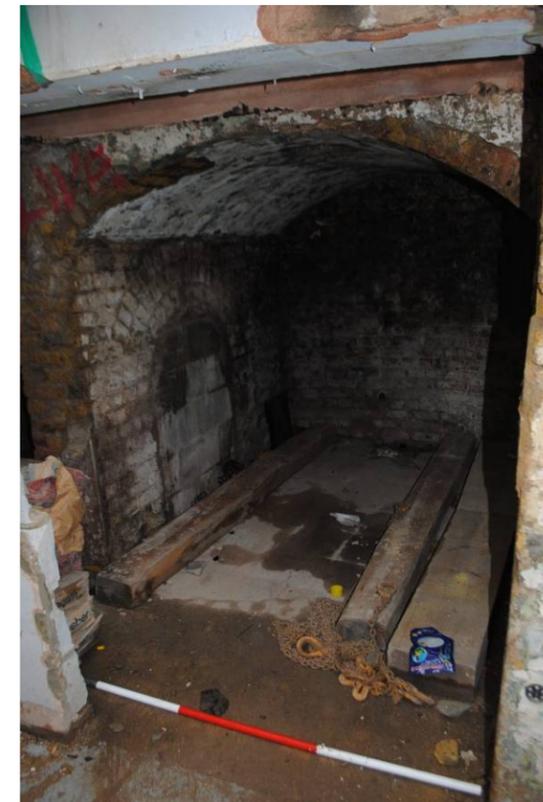


Plate 75 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 76 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 78 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 77 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 79 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 80 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 82 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 81 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 83 - 20-23 Long Lane and 22 Lindsey Street, vault detail post demolition



Plate 84 - 20-23 Long Lane and 22 Lindsey Street, splayed light well post demolition



Plate 86 - 20-23 Long Lane and 22 Lindsey Street, modern stairwell detail

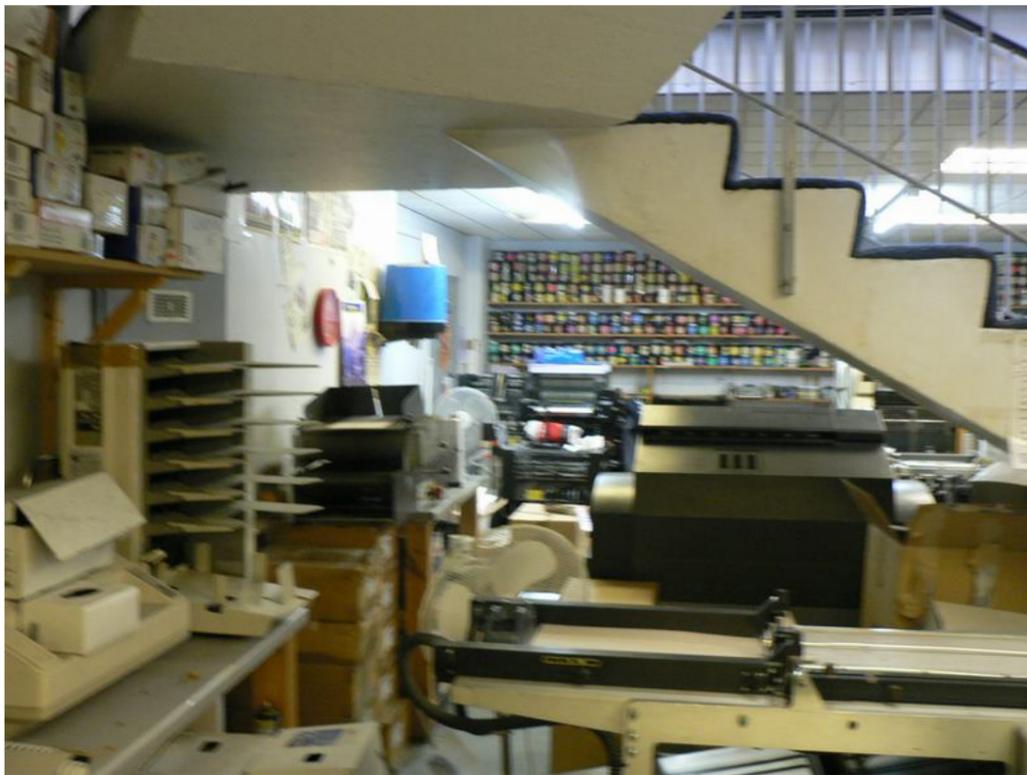


Plate 85 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 87 - 20-23 Long Lane and 22 Lindsey Street, showing modern fittings



Plate 88 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 89 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 90 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 91 - 20-23 Long Lane and 22 Lindsey Street, view toward roof light



Plate 92 - 20-23 Long Lane and 22 Lindsey Street, roof light detail

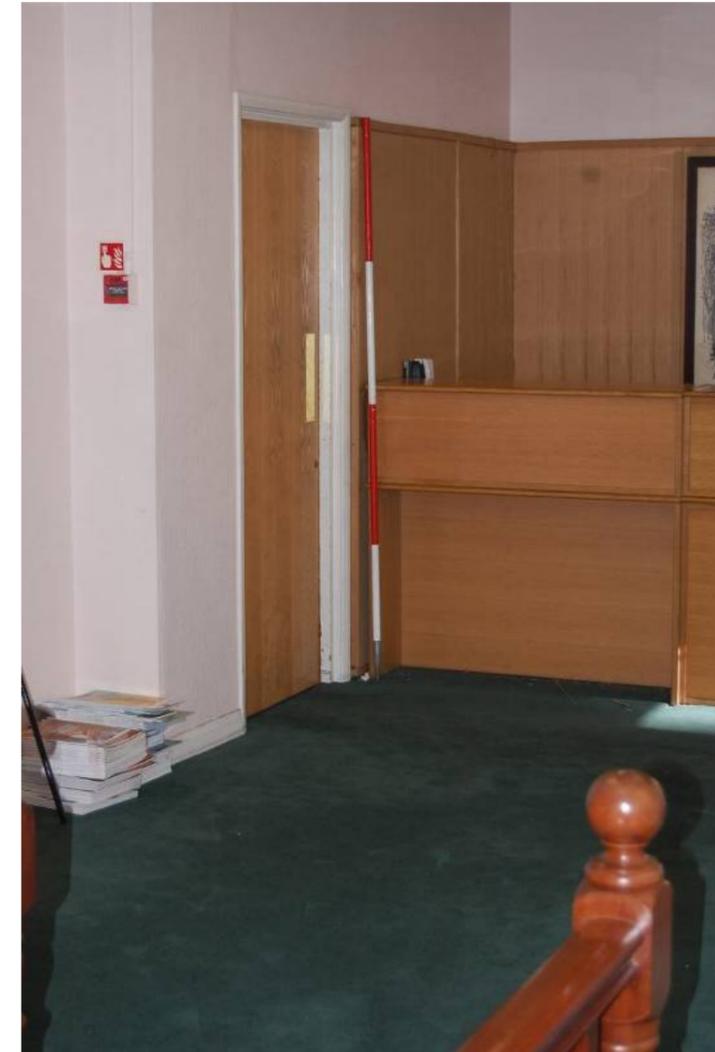


Plate 94 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 93 - 20-23 Long Lane and 22 Lindsey Street, general view

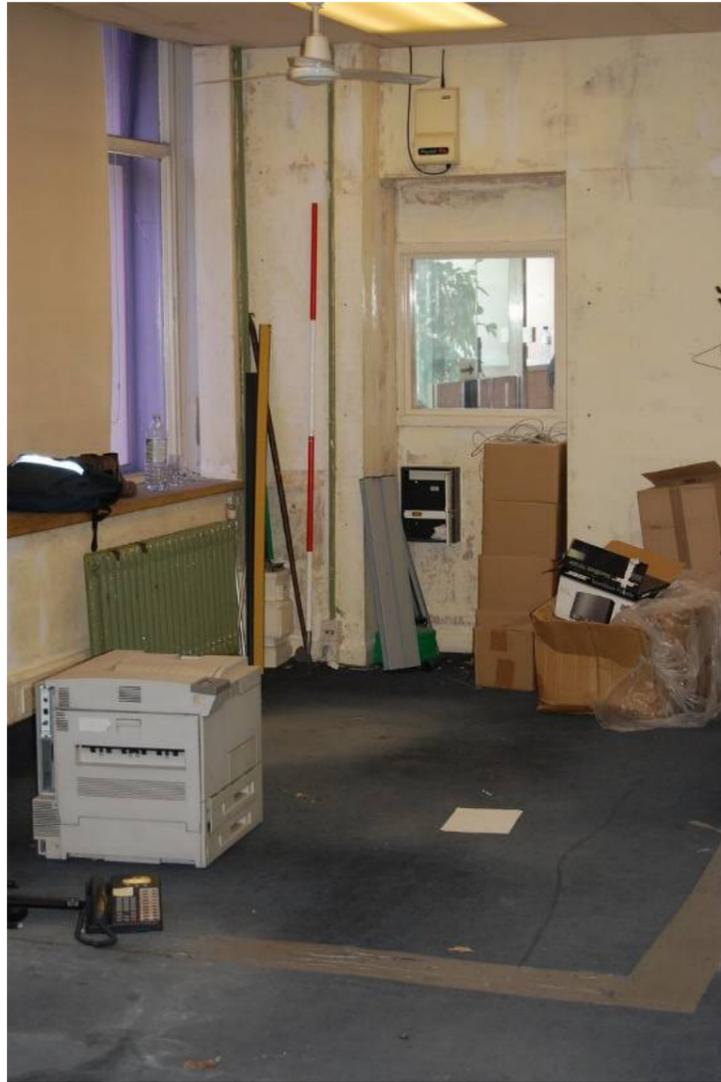


Plate 95 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 96 - 20-23 Long Lane and 22 Lindsey Street, modern inserted room



Plate 97 - 20-23 Long Lane and 22 Lindsey Street, stair detail



Plate 98 - 20-23 Long Lane and 22 Lindsey Street, entrance detail



Plate 99 - 20-23 Long Lane and 22 Lindsey Street, stair detail



Plate 100 - 20-23 Long Lane and 22 Lindsey Street, lift detail



Plate 101 - 20-23 Long Lane and 22 Lindsey Street, stair detail



Plate 103 - 20-23 Long Lane and 22 Lindsey Street, door detail



Plate 102 - 20-23 Long Lane and 22 Lindsey Street, view of open space



Plate 104 - 20-23 Long Lane and 22 Lindsey Street, general view



Plate 106 - 20-23 Long Lane and 22 Lindsey Street, view of open space with visible stanchions



Plate 105 - 20-23 Long Lane and 22 Lindsey Street, view of open space



Plate 107 - 20-23 Long Lane and 22 Lindsey Street, inserted doorway



Plate 108 - 20-23 Long Lane and 22 Lindsey Street, stairs to flat roof



Plate 109 - 20-23 Long Lane and 22 Lindsey Street, 1940's lift door



**Plate 110 - 20-23 Long Lane and 22 Lindsey Street, roof structure**



**Plate 111 – 20-23 Long Lane and 22 Lindsey Street, hoist detail**

3 Lindsey Street



Plate 112 - 3 Lindsey Street, facade



Plate 113 - 3 Lindsey Street, rear view showing main body of structure surrounded by lower toilet block



Plate 114 - 3 Lindsey Street, surviving pilaster from the 1916 GWR goods office



Plate 115 - 3 Lindsey Street, ceramic tile detail



Plate 116 - 3 Lindsey Street, access to the platform level was via 4 Lindsey Street to the right



Plate 117 - 3 Lindsey Street, jack arch ceiling and lift shaft to 8-9 Hayne Street



Plate 118 - 3 Lindsey Street, small aperture



Plate 120 - 3 Lindsey Street, former storage room



Plate 119 - 3 Lindsey Street, partially blocked archway that originally provided access to sidings



Plate 121 - 3 Lindsey Street, jack arch ceiling detail



Plate 122 - 3 Lindsey Street, former storage room access door



Plate 124 - 3 Lindsey Street, former storage room



Plate 123 - 3 Lindsey Street, small aperture



Plate 125 - 3 Lindsey Street, former storage room



Plate 126 - 3 Lindsey Street, partially blocked archway that originally provided access to sidings



Plate 128- 3 Lindsey Street, view to basement level from platform level



Plate 127 - 3 Lindsey Street, windows of former Men's Mess Room



Plate 129 - 3 Lindsey Street, view of block doorways that previously provided access to sidings and offices



Plate 131 - 3 Lindsey Street, flue opening to platform level



Plate 130 - 3 Lindsey Street, blocked up arches to platform level



Plate 132 - 3 Lindsey Street, view towards vaulted room under Lindsey Street



Plate 133 - 3 Lindsey Street, rail tracks reused in wall



Plate 135 - 3 Lindsey Street, basement level



Plate 134 - 3 Lindsey Street, basement level with modern wall extant



Plate 136 - 3 Lindsey Street, basement level



Plate 137 - 3 Lindsey Street, toilet block



Plate 139 - 3 Lindsey Street, ground floor interior



Plate 138 - 3 Lindsey Street, toilet block



Plate 140 - 3 Lindsey Street, ground floor interior



Plate 141 - 3 Lindsey Street, ladder access to mezzanine level



Plate 142 - 3 Lindsey Street, early moulded shop surround



Plate 143 - 3 Lindsey Street, view up to mezzanine level



Plate 144 - 3 Lindsey Street, ceiling detail



Plate 145 - 3 Lindsey Street, fink trusses and lights to roof above



Plate 146 - 3 Lindsey Street, ladder detail



Plate 147 – 3 Lindsey Street, wash room

**Chimney to the rear of 20-21 Long Lane and the retaining arches below Hayne Street 54-64  
Charterhouse Square**



**Plate 148 - Chimney and retaining walls, chimney detail**



**Plate 149 - Chimney and retaining walls, plinth to basement of chimney**



**Plate 150 - Chimney and retaining walls, view of chimney summit**



Plate 151 - Chimney and retaining walls, modern access to base of chimney from basement of 3 Lindsey Street



Plate 152 - Chimney and retaining walls, looking up shaft of chimney



Plate 153 - Chimney and retaining walls, note void at intersection where chimney has been demolished



Plate 154 - Chimney and retaining walls, note void at intersection where chimney has been demolished



Plate 155 - Chimney and retaining walls, note void at intersection where chimney has been demolished



Plate 157 - Chimney and retaining walls, eastern wall detail



Plate 156 - Chimney and retaining walls, eastern wall detail



Plate 158 - Chimney and retaining walls, eastern wall detail, showing boarded up door above that linked 8-9 Hayne Street with the former GWR building



Plate 159 - Chimney and retaining walls, eastern wall detail

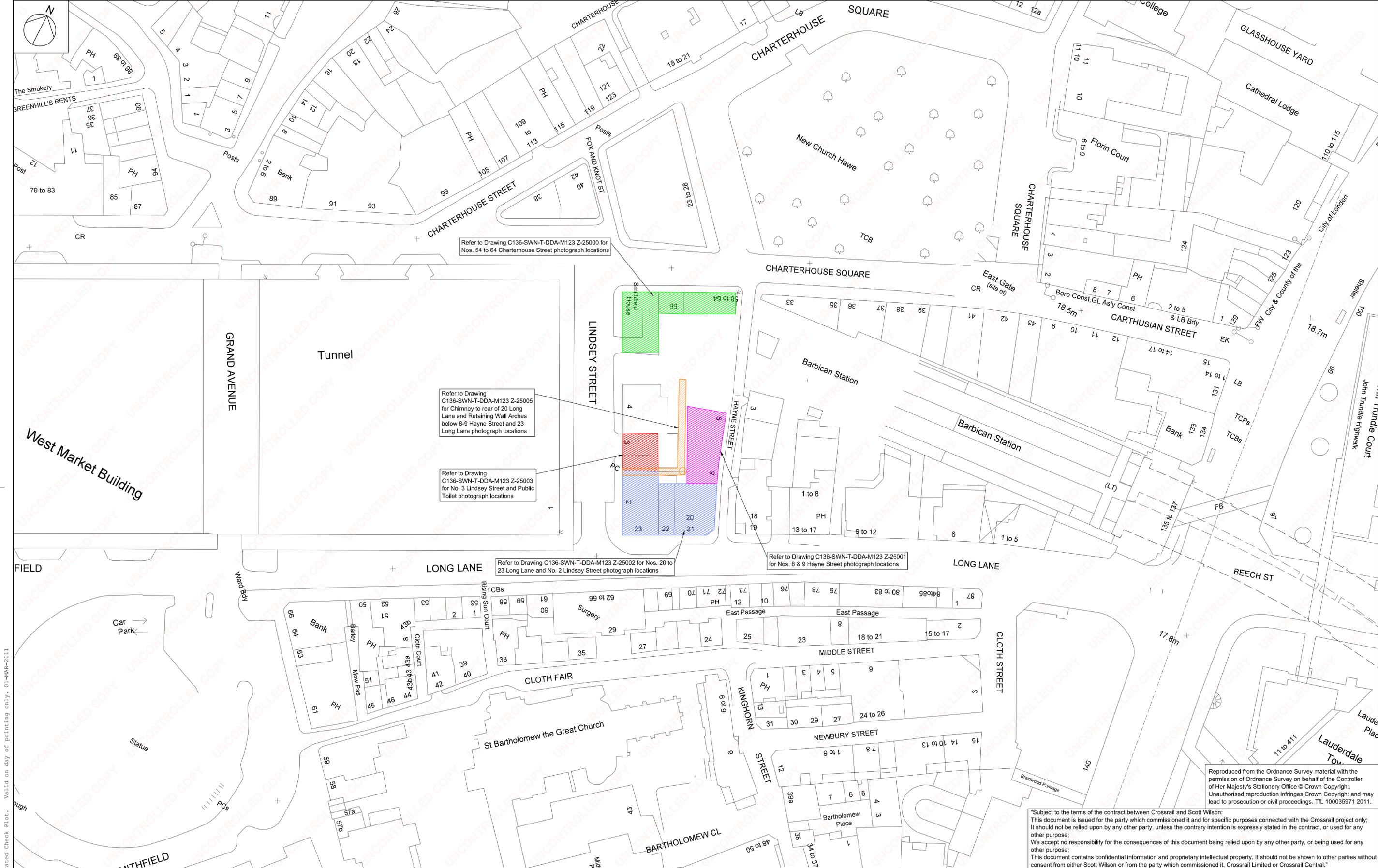


Plate 160 – Chimney and retaining walls, former lift shaft to 8-9 Hayne Street

## Appendix 2 – Site location plan and photographic location plans

Title	Drawing No.
Location plan	C136-SWN-T-DDA-M123_Z-25005
54-64 Charterhouse Street	C136-SWN-T-DDA-M123_Z-25000
8-9 Hayne Street	C136-SWN-T-DDA-M123_Z-25001
20-23 Long Lane	C136-SWN-T-DDA-M123_Z-25002
3 Lindsey Street / Public toilets	C136-SWN-T-DDA-M123_Z-25003
Chimney to the rear of No. 20 Long Lane and retaining arches below Nos. 8-9 Hayne Street and No. 23 Long Lane	C136-SWN-T-DDA-M123_Z-25004

**Table 1 – List of attached drawings**



Refer to Drawing C136-SWN-T-DDA-M123 Z-25000 for Nos. 54 to 64 Charterhouse Street photograph locations

Refer to Drawing C136-SWN-T-DDA-M123 Z-25005 for Chimney to rear of 20 Long Lane and Retaining Wall Arches below 8-9 Hayne Street and 23 Long Lane photograph locations

Refer to Drawing C136-SWN-T-DDA-M123 Z-25003 for No. 3 Lindsey Street and Public Toilet photograph locations

Refer to Drawing C136-SWN-T-DDA-M123 Z-25002 for Nos. 20 to 23 Long Lane and No. 2 Lindsey Street photograph locations

Refer to Drawing C136-SWN-T-DDA-M123 Z-25001 for Nos. 8 & 9 Hayne Street photograph locations

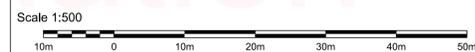
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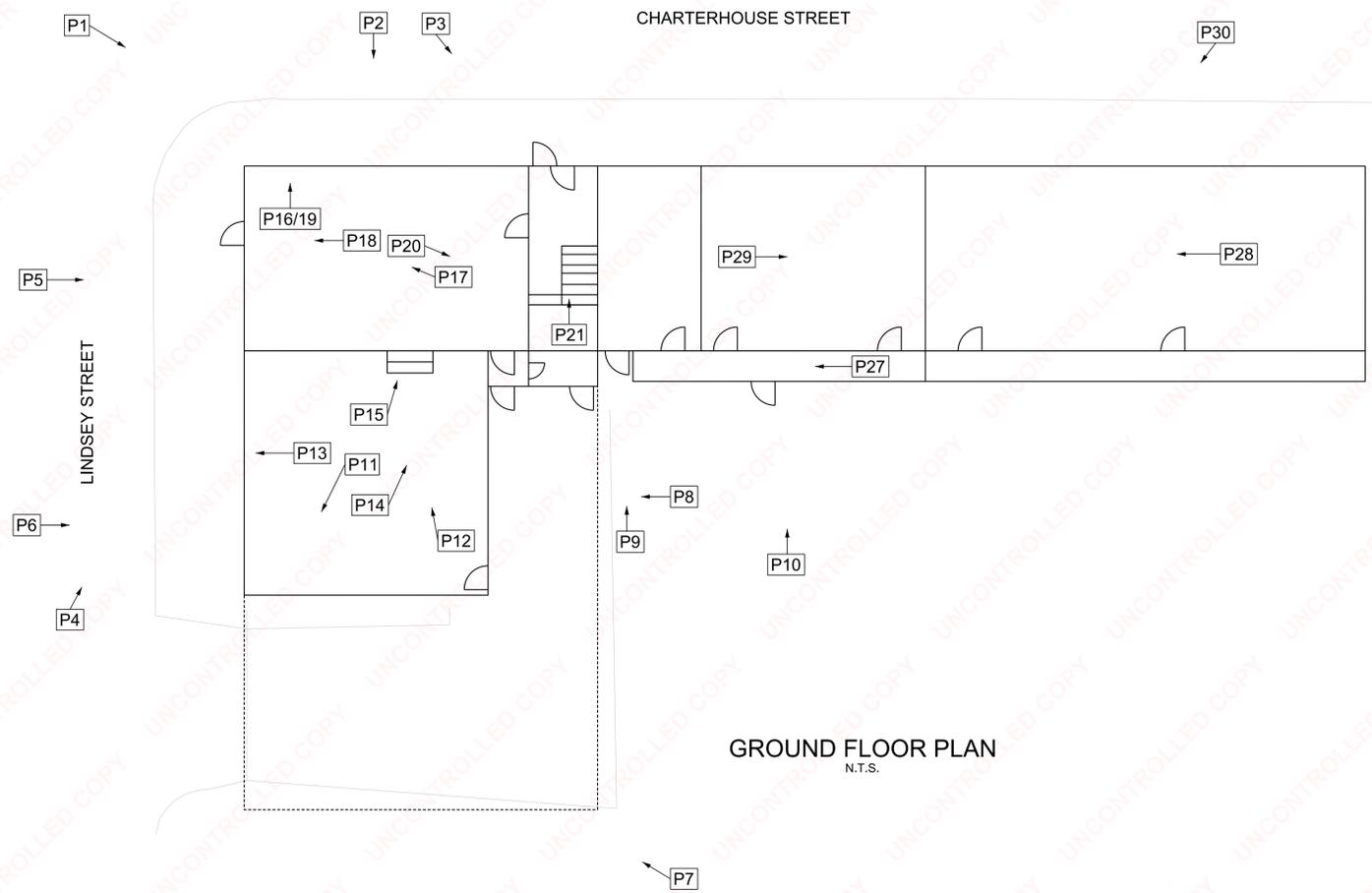
Rev.	Date	Description	By	Chkd	App	Auth
P01	01/03/2011	First Issue	AD	MC	---	---

Notes

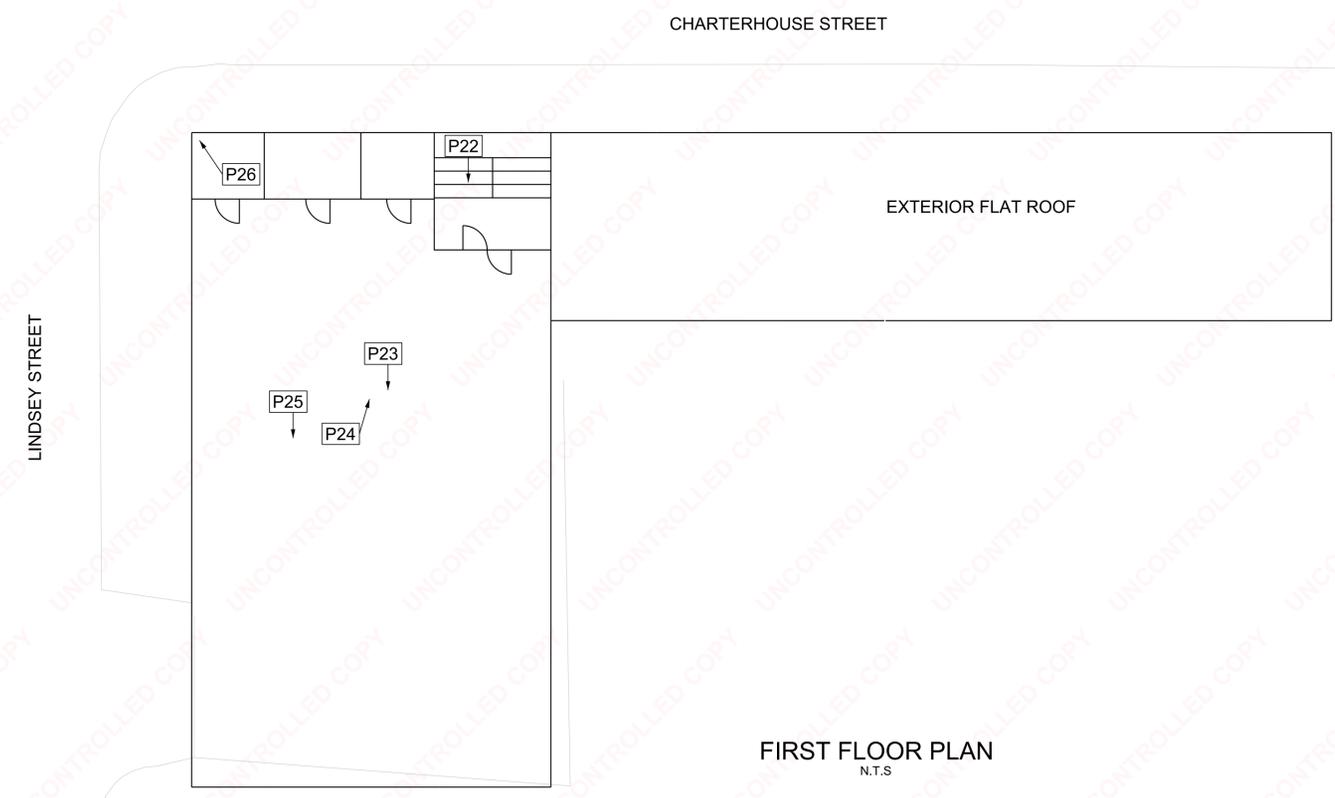


	<b>Farringdon Station Design</b> Originator: <b>Scott Wilson Limited</b> Location: Farringdon Stn	By: A.DOYLE Chk: M.COURT App: --- Auth: ---
	Crossrail Limited 25 Canada Square London E14 5LQ	<b>Eastern Entrance          Location Plan          Photograph Locations</b>
© Crossrail www.crossrail.co.uk	Scale: <b>1:500@A1</b>	Drawing and CAD file No: <b>C136-SWN-T-DDA-M123_Z-25005</b>

RESTRICTED



GROUND FLOOR PLAN  
N.T.S.



FIRST FLOOR PLAN  
N.T.S.

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Notes

Key:

P1 → Shows photograph reference number and direction of camera.

P1 → Green = Photograph taken post demolition

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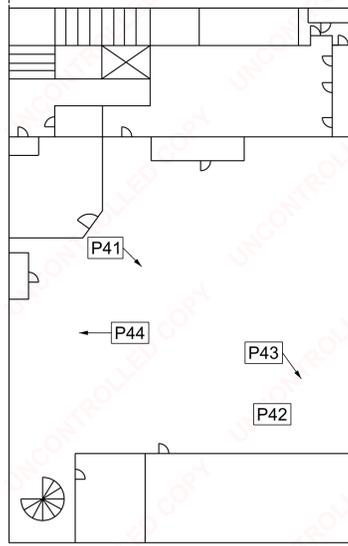
	<b>Farringdon Station Design</b> Originator: <b>Scott Wilson Limited</b>		Title: <b>Eastern Entrance Photograph Locations</b>	By: A.DOYLE
	Location: <b>Farringdon Stn</b>		App: ... Auth: ...	Scale: <b>NTS@ A1</b>
Contract: <b>25 Canada Square London E14 5LQ</b>	Drawing and CAD file No.: <b>C136-SWN-T-DDA-M123_Z-25000</b>		Rev: <b>P01.2</b>	Suitability: <b>S0</b>
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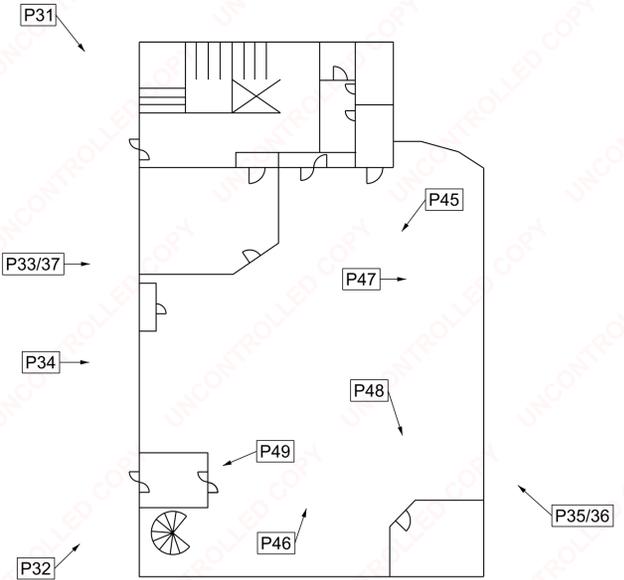


HAYNE STREET

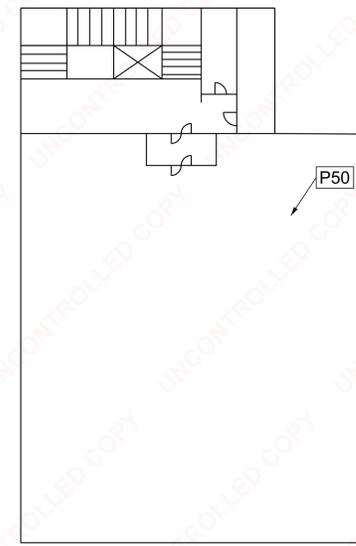
20-23 LONG LANE



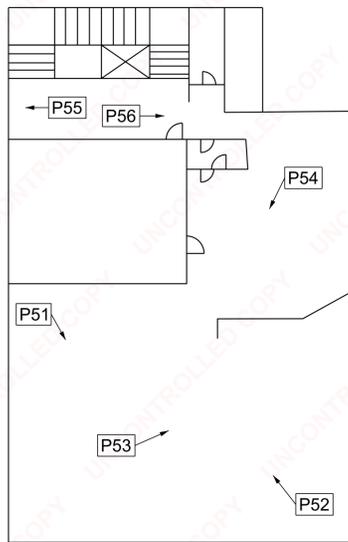
BASEMENT



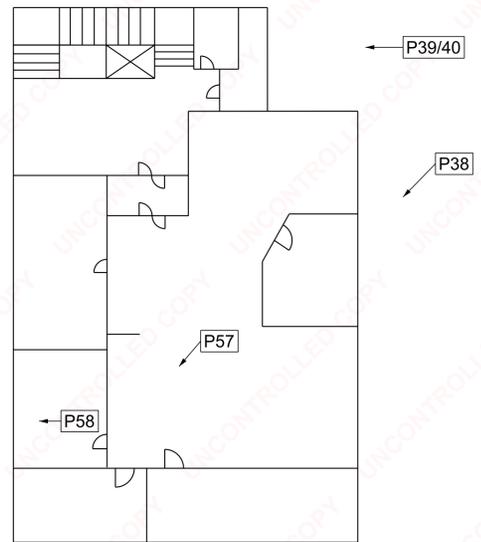
GROUND FLOOR



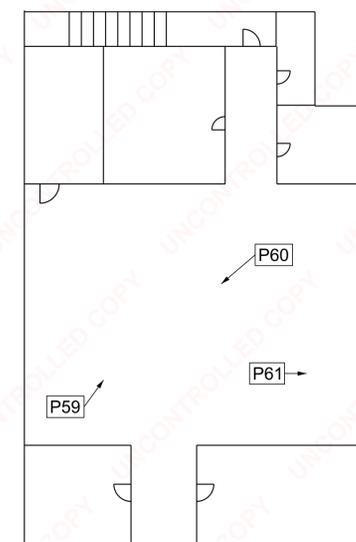
FIRST FLOOR



SECOND FLOOR



THIRD FLOOR



FOURTH FLOOR

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Rev.	Date	Description	By	Chkd	App	Auth
P01.1	28/02/2011	First Issue	AD	---	---	---

Notes  
Key:

Shows photograph reference number and direction of camera.

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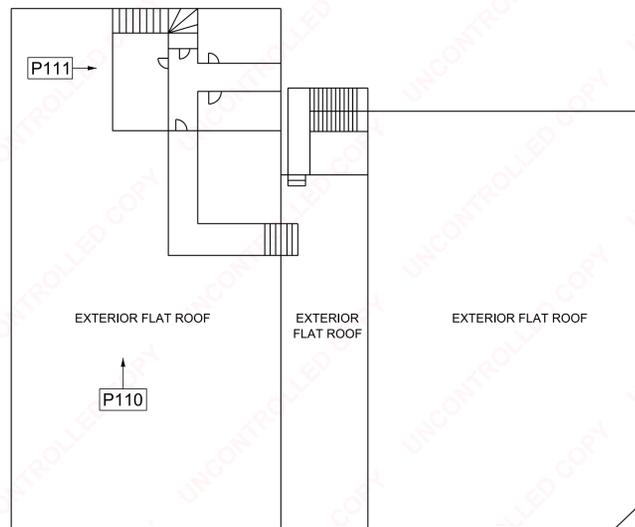
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	Originator:	Scott Wilson Limited	
	Location:	Farringdon Stn	App: ...
	Title:	Eastern Entrance Photograph Locations 8-9 Hayne Street	Auth: ...
Scale:	1:200@ A1	Drawing and CAD file No:	Rev: P01.1
www.crossrail.co.uk		C136-SWN-T-DDA-M123_Z-25001	Suitability: S0

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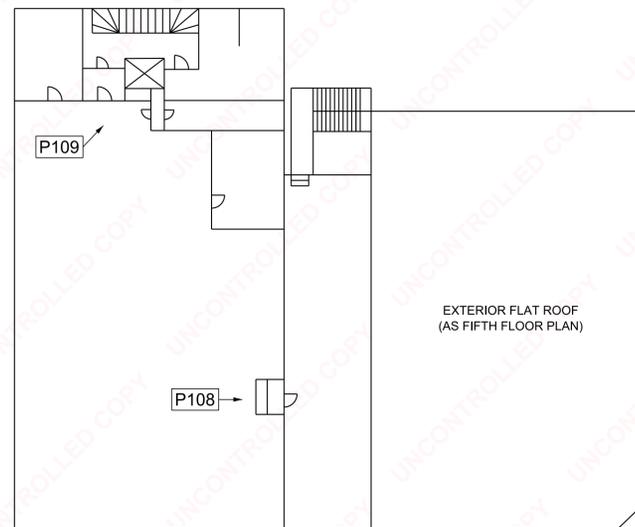
PDF Check Plot

PDF Check Plot

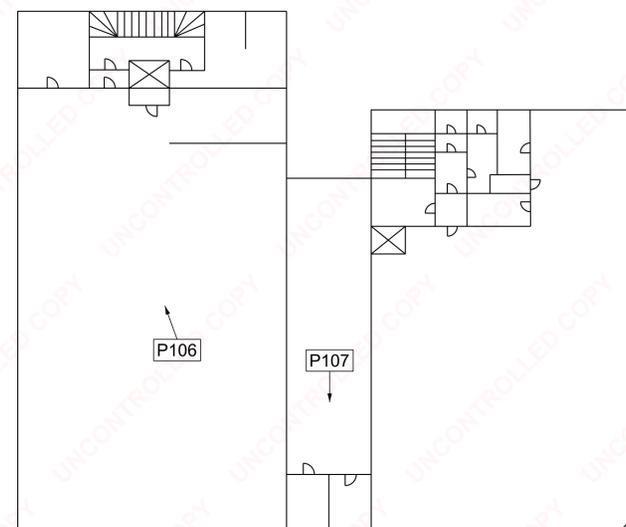
RESTRICTED



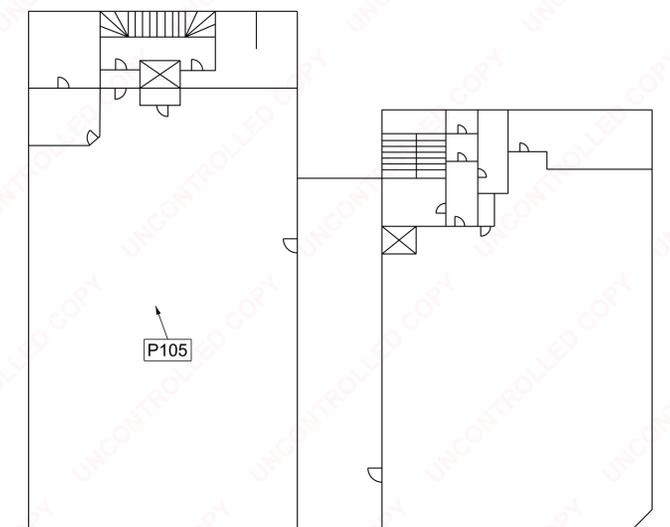
FIFTH FLOOR AND EXTERIOR ROOF AREA



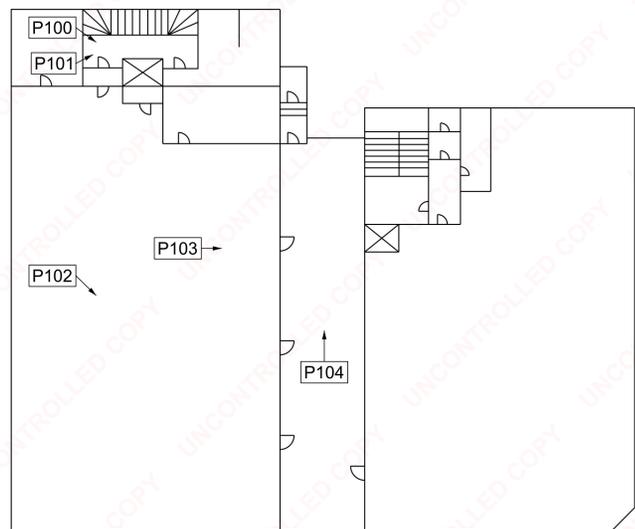
FOURTH FLOOR AND EXTERIOR ROOF AREA



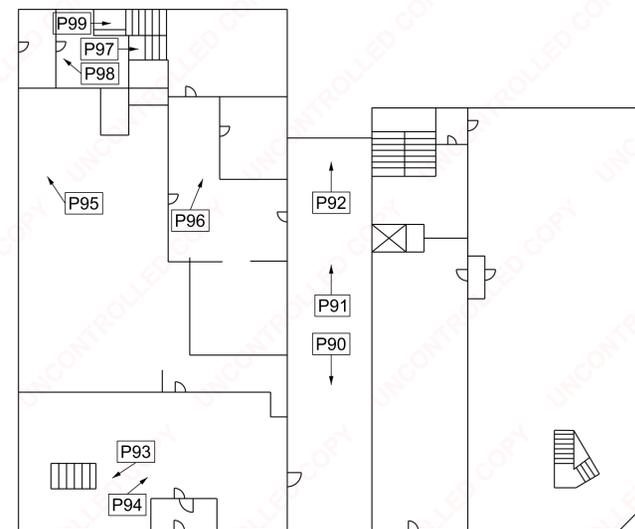
THIRD FLOOR



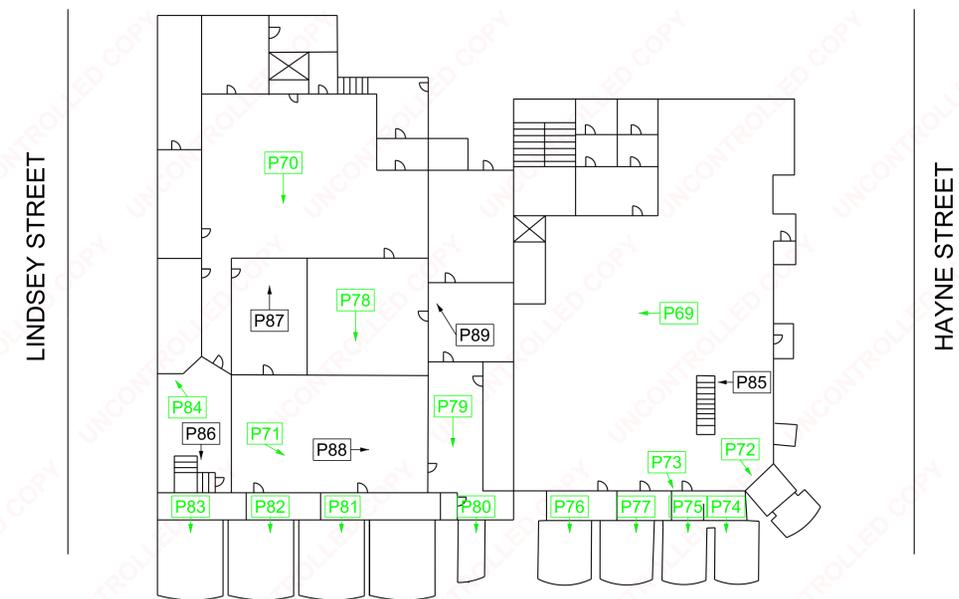
SECOND FLOOR



FIRST FLOOR



GROUND FLOOR



BASEMENT

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Key:

**P1** → Shows photograph reference number and direction of camera.

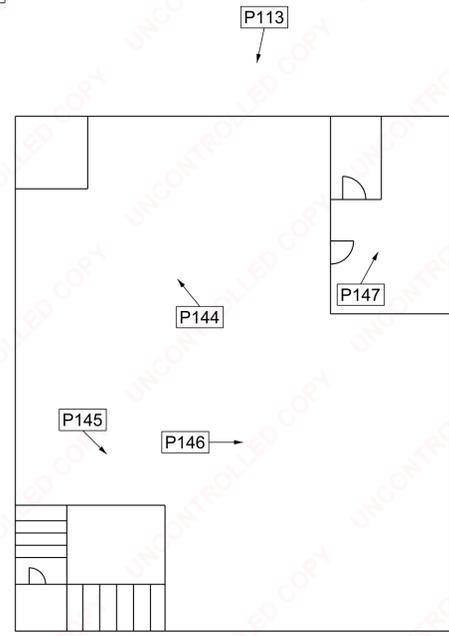
**P1** → Green = Photograph taken post demolition

<p>Crossrail Limited 25 Canada Square London E14 5LQ</p>	<p>Contract: Farringdon Station Design Originator: Scott Wilson Limited Location: Farringdon Stn</p>	<p>By: ADOYLE Chk: --- App: --- Auth: ---</p>
	<p>Title: Eastern Entrance Photograph Locations Nos. 20-23 Long Lane &amp; No. 2 Lindsey Street</p>	<p>Scale: NTS@ A1 Drawing and CAD file No.: C136-SWN-T-DDA-M123_Z-25002 Rev: P01.1 Suitability: S0</p>

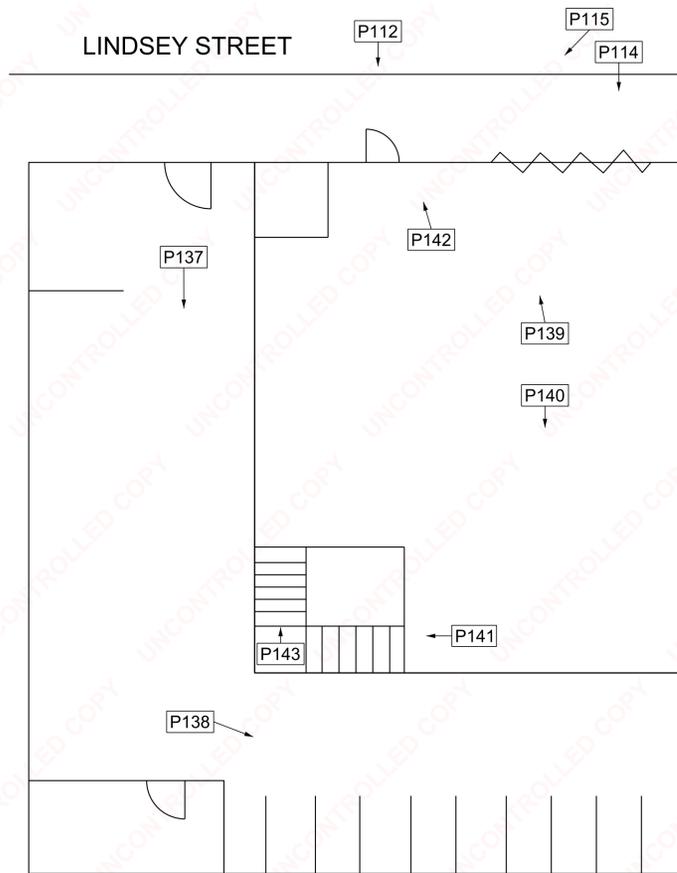
PDF Check Plot

PDF Check Plot

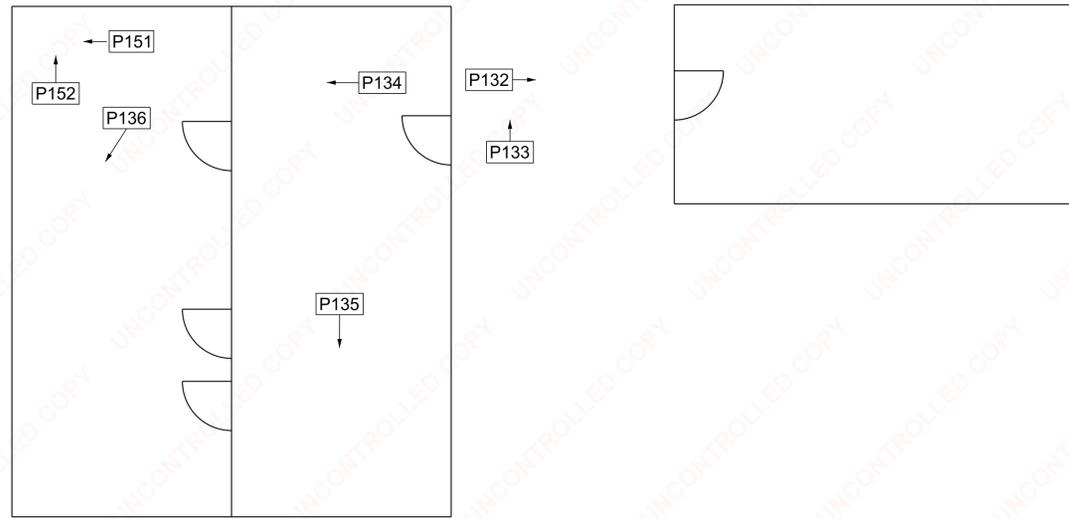
RESTRICTED



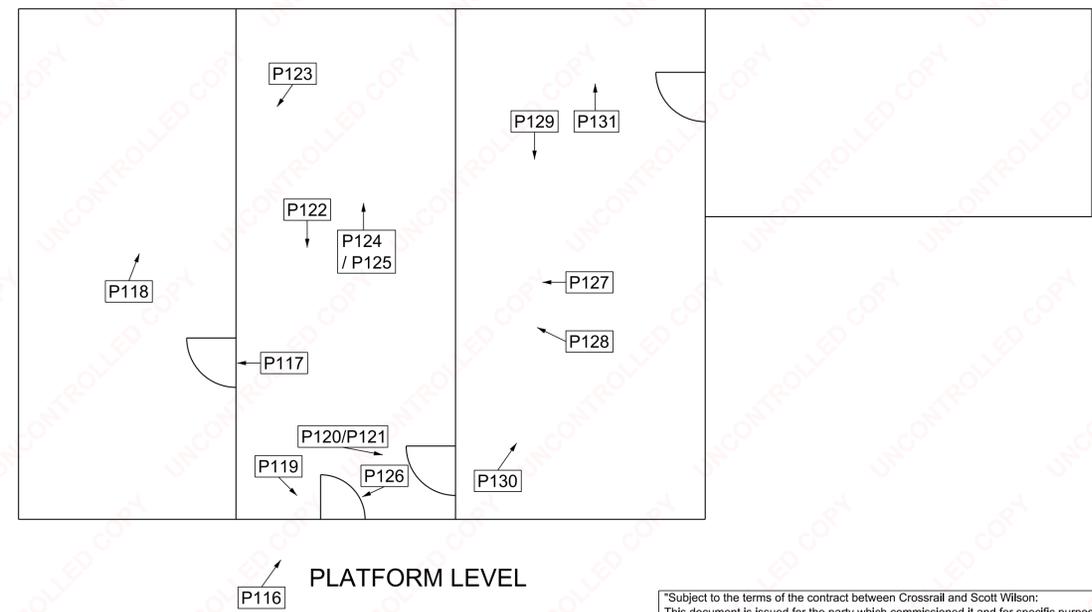
FIRST FLOOR



GROUND FLOOR



BASEMENT LEVEL 1



PLATFORM LEVEL

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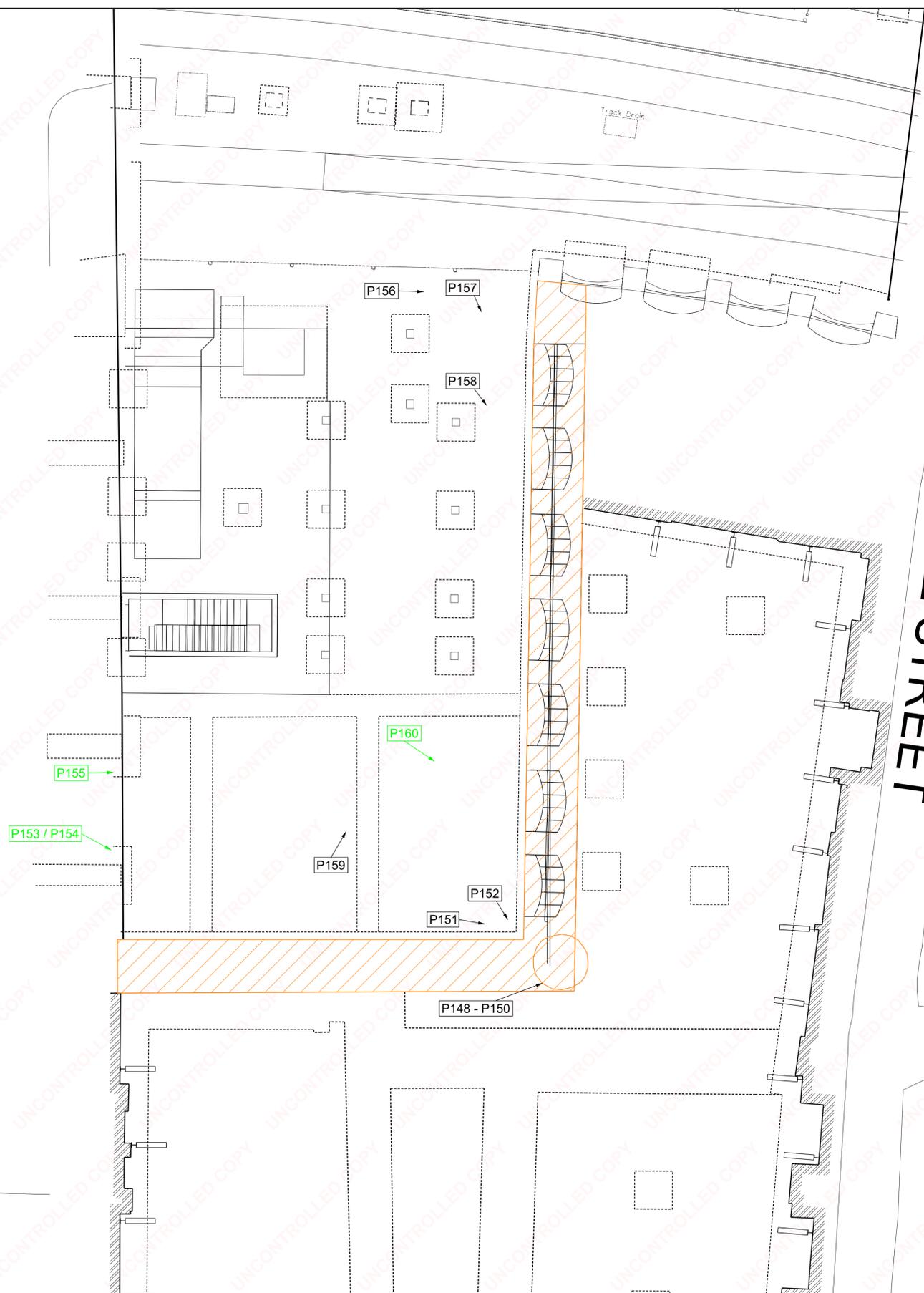
Rev.	Date	Description	By	Chkd	App	Auth
P01.2	01/03/2011	First Issue	AD	---	---	---

Notes  
 Key:  
 [P1] → Shows photograph reference number and direction of camera.  
 [P1] → Green = Photograph taken post demolition

	<b>Farringdon Station Design</b> Originator: <b>Scott Wilson Limited</b> Location: <b>Farringdon Stn</b>	By: ADOYLE Chk: ... App: ... Auth: ...
	Crossrail Limited 25 Canada Square London E14 5LQ	<b>Eastern Entrance</b> <b>Photograph Locations</b> <b>3 Lindsey Street &amp; Public Toilets</b>
© Crossrail www.crossrail.co.uk	Scale: NTS@ A1 Drawing and CAD file No: C136-SWN-T-DDA-M123_Z-25003	Rev: P01.2 Suitability: S0



# SEY STREET



# HAYNE STREET

3

1

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Rev.	Date	Description	By	Chkd	App	Auth
P01.1	28/02/2011	First Issue	AD	---	---	---

Notes  
 Key:  
 P1 → Shows photograph reference number and direction of camera.  
 P1 → Green = Photograph taken post demolition



Farringdon Station Design  
 Originator:  
 Scott Wilson Limited  
 Location:  
 Farringdon Stn  
 Title:  
 Eastern Entrance  
 Photograph Locations  
 Chimney to Rear of 20 Long Lane and Retaining  
 Wall Arches Below 8-9 Hayne Street and 23 Long Lane  
 By: ADOYLE  
 CHK: ...  
 App: ...  
 Auth: ...  
 Scale: 1:200@ A1  
 Drawing and CAD file No: C136-SWN-T-DDA-M123\_Z-25004  
 Rev: P01.1  
 Suitability: S0

PDF Check Plot  
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**Appendix 3 Photographic register**

PHOTOGRAPHIC REGISTER					
FILM NO.	1	NGR	TQ 319 818	FILM TYPE	35mm
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400
Frame No.	Building	Description	Direction	Date	Photographer
1	Smithfield House	View of Façade	S	21/03/2010	IM
2	Smithfield House	View of single storey structure	SE	21/03/2010	IM
3	Smithfield House	Detail of 1st floor windows	S	21/03/2010	IM
4	Smithfield House	Detail of 1st floor stair window	S	21/03/2010	IM
5	Smithfield House	View of western elevation	NE	21/03/2010	IM
6	Smithfield House	Detail of 1st floor windows	NE	21/03/2010	IM
7	Smithfield House	Detail of 1st floor windows	NE	21/03/2010	IM
8	8-9 Hayne Street	View of northern elevation	S	21/03/2010	IM
9	8-9 Hayne Street	View of eastern and northern elevations	S	21/03/2010	IM
10	8-9 Hayne Street	View of eastern elevation	NW	21/03/2010	IM
11	8-9 Hayne Street	View of central doorway	W	21/03/2010	IM
12	8-9 Hayne Street	View of plaques	W	21/03/2010	IM
13	8-9 Hayne Street	View of ground floor window	W	21/03/2010	IM
14	8-9 Hayne Street	View of altered 1st floor window	W	21/03/2010	IM
15	8-9 Hayne Street	View of 1st floor window	W	21/03/2010	IM
16	8-9 Hayne Street	Internal view of front door	E	21/03/2010	IM
17	8-9 Hayne Street	View of basement stanchions	NW	21/03/2010	IM
18	8-9 Hayne Street	View of basement stanchions	NW	21/03/2010	IM
19	8-9 Hayne Street	View of basement	S	21/03/2010	IM
20	8-9 Hayne Street	View of basement window	E	21/03/2010	IM
21	8-9 Hayne Street	View of ground floor	N	21/03/2010	IM
22	8-9 Hayne Street	View of ground floor	S	21/03/2010	IM
23	8-9 Hayne Street	Detail of window ground floor	W	21/03/2010	IM
24	8-9 Hayne Street	Stanchion to ground floor	NW	21/03/2010	IM
25	8-9 Hayne Street	Internal view of 1st floor	N	21/03/2010	IM
26	3 Lindsey Street	View of façade	E	21/03/2010	IM
27	3 Lindsey Street	View of tile work to façade	E	21/03/2010	IM
28	3 Lindsey Street	View of louvre above façade	E	21/03/2010	IM
29	3 Lindsey Street	View of fluted pilaster	E	21/03/2010	IM

30	3 Lindsey Street	Internal view of toilet block	E	21/03/2010	IM
31	3 Lindsey Street	Toilet detail	S	21/03/2010	IM
32	3 Lindsey Street	Internal view of toilet block	N	21/03/2010	IM
33	3 Lindsey Street	Internal view of ground floor	W	21/03/2010	IM
34	3 Lindsey Street	Internal view of ground floor	E	21/03/2010	IM
35	3 Lindsey Street	Internal view of stair access to first floor	S	21/03/2010	IM
36	3 Lindsey Street	Internal view of first floor	W	21/03/2010	IM

**PHOTOGRAPHIC REGISTER**

FILM NO.	2	NGR	TQ 319 818	FILM TYPE	35mm
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400

Frame No.	Building	Description	Direction	Date	Photographer
1	Smithfield House	View of rear elevation	W	21/03/2010	IM
2	Smithfield House	View of rear single storey	N	21/03/2010	IM
3	Smithfield House	View of rear single storey	N	21/03/2010	IM
4	Smithfield House	Internal ground floor view	S	21/03/2010	IM
5	Smithfield House	Internal ground floor view	N	21/03/2010	IM
6	Smithfield House	Internal door detail	W	21/03/2010	IM
7	Smithfield House	Ceiling detail	-	21/03/2010	IM
8	8-9 Hayne Street	Ground floor view	E	21/03/2010	IM
9	8-9 Hayne Street	Ground floor view	W	21/03/2010	IM
10	8-9 Hayne Street	Ground floor door detail	W	21/03/2010	IM
11	8-9 Hayne Street	Ground floor window detail	N	21/03/2010	IM
12	8-9 Hayne Street	Stair detail	S	21/03/2010	IM
13	8-9 Hayne Street	Window detail to stair	N	21/03/2010	IM
14	8-9 Hayne Street	First floor view	SE	21/03/2010	IM
15	8-9 Hayne Street	First floor view	NE	21/03/2010	IM
16	8-9 Hayne Street	First floor truss detail	SE	21/03/2010	IM
17	8-9 Hayne Street	First floor truss detail	SE	21/03/2010	IM
18	8-9 Hayne Street	First floor window detail	W	21/03/2010	IM
19	8-9 Hayne Street	First floor window detail	NW	21/03/2010	IM
20	8-9 Hayne Street	Single storey structure internal view	W	21/03/2010	IM
21	8-9 Hayne Street	Single storey structure internal view	E	21/03/2010	IM
22	20-23 Long Lane	External view from Lindsey Street	E	21/03/2010	IM
23	20-23 Long Lane	External view from Long Lane	N	21/03/2010	IM
24	20-23 Long Lane	External view from Long Lane	N	21/03/2010	IM
25	20-23 Long Lane	First floor view	N	21/03/2010	IM
26	20-23 Long Lane	Chimney detail to roof	E	21/03/2010	IM
27	20-23 Long Lane	View of roof	W	21/03/2010	IM
28	20-23 Long Lane	View of hoist door	N	21/03/2010	IM
29	20-23 Long Lane			21/03/2010	IM
30	20-23 Long Lane			21/03/2010	IM

31	20-23 Long Lane			21/03/2010	IM
32	20-23 Long Lane			21/03/2010	IM
33	20-23 Long Lane			21/03/2010	IM
34	20-23 Long Lane			21/03/2010	IM
35	20-23 Long Lane			21/03/2010	IM
36	20-23 Long Lane			21/03/2010	IM

PHOTOGRAPHIC REGISTER					
FILM NO.	3	NGR	TQ 319 818	FILM TYPE	35mm
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400
Frame No.	Building	Description	Direction	Date	Photographer
1	20-23 Long Lane	Modern Stair detail to basement	S	21/03/2010	IM
2	20-23 Long Lane	View of basement	N	21/03/2010	IM
3	20-23 Long Lane	View of basement	E	21/03/2010	IM
4	20-23 Long Lane	Detail of machinery in basement	NW	21/03/2010	IM
5	20-23 Long Lane	View of ground floor	SW	21/03/2010	IM
6	20-23 Long Lane	View of ground floor	NE	21/03/2010	IM
7	20-23 Long Lane	View of ground floor	NW	21/03/2010	IM
8	20-23 Long Lane	Ground floor, modern inserted room	NE	21/03/2010	IM
9	20-23 Long Lane	View of stairwell towards basement	E	21/03/2010	IM
10	20-23 Long Lane	Ground floor entrance detail	NW	21/03/2010	IM
11	20-23 Long Lane	View of stairwell towards first floor	E	21/03/2010	IM
12	20-23 Long Lane	First floor lift detail	SE	21/03/2010	IM
13	20-23 Long Lane	First floor banister detail	N	21/03/2010	IM
14	20-23 Long Lane	First floor banister detail	N	21/03/2010	IM
15	20-23 Long Lane	View of first floor	SE	21/03/2010	IM
16	20-23 Long Lane	Door detail first floor	E	21/03/2010	IM
17	20-23 Long Lane	Second floor view	NW	21/03/2010	IM
18	20-23 Long Lane	Third floor view	NW	21/03/2010	IM
19	20-23 Long Lane	Third floor view	S	21/03/2010	IM
20	20-23 Long Lane	Fourth floor view towards flat roof	E	21/03/2010	IM
21	20-23 Long Lane	Lift door to fourth floor	NE	21/03/2010	IM
22	20-23 Long Lane			21/03/2010	IM
23	20-23 Long Lane			21/03/2010	IM
24	20-23 Long Lane			21/03/2010	IM
25	20-23 Long Lane			21/03/2010	IM
26	20-23 Long Lane			21/03/2010	IM
27	20-23 Long Lane			21/03/2010	IM
28	20-23 Long Lane			21/03/2010	IM
29	20-23 Long Lane			21/03/2010	IM
30	20-23 Long Lane			21/03/2010	IM

31	20-23 Long Lane			21/03/2010	IM
32	20-23 Long Lane			21/03/2010	IM
33	20-23 Long Lane			21/03/2010	IM
34	20-23 Long Lane			21/03/2010	IM
35	20-23 Long Lane			21/03/2010	IM
36	20-23 Long Lane			21/03/2010	IM

PHOTOGRAPHIC REGISTER					
FILM NO.	4	NGR	TQ 319 818	FILM TYPE	35mm
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400
Frame No.	Building	Description	Direction	Date	Photographer
1	20-23 Long Lane	View of northern elevation	S	21/03/2010	IM
2	20-23 Long Lane	View of north eastern elevation	SW	21/03/2010	IM
3	20-23 Long Lane	View of ground floor room	S	21/03/2010	IM
4	20-23 Long Lane	Roof light detail	-	21/03/2010	IM
5	20-23 Long Lane	View of ground floor room	N	21/03/2010	IM
6	8-9 Hayne Street	Rear elevation	NE	21/03/2010	IM
7	8-9 Hayne Street	Lean to to rear elevation	NE	21/03/2010	IM
8	8-9 Hayne Street	Rear elevation	NE	21/03/2010	IM
9	8-9 Hayne Street	Blocked up door to rear elevation	NE	21/03/2010	IM
10	8-9 Hayne Street	Second floor general view	NW	21/03/2010	IM
11	8-9 Hayne Street	Second floor general view	SE	21/03/2010	IM
12	8-9 Hayne Street	Second floor cast iron column detail	W	21/03/2010	IM
13	8-9 Hayne Street	Second floor, general view	N	21/03/2010	IM
14	8-9 Hayne Street	Second floor, window detail	E	21/03/2010	IM
15	20-23 Long Lane	Second floor radiator detail	E	21/03/2010	IM
16	20-23 Long Lane	Second floor landing detail	W	21/03/2010	IM
17	20-23 Long Lane	Second floor modern lift door	S	21/03/2010	IM
18	20-23 Long Lane	View of stairwell towards third floor	S	21/03/2010	IM
19	20-23 Long Lane	Third floor general view	N	21/03/2010	IM
20	20-23 Long Lane	Third floor column detail	N	21/03/2010	IM
21	20-23 Long Lane	Window details to third floor	E	21/03/2010	IM
22	20-23 Long Lane	Stair detail to fourth floor	N	21/03/2010	IM
23	20-23 Long Lane	Fourth floor general view	N	21/03/2010	IM
24	20-23 Long Lane	Fourth floor general view	S	21/03/2010	IM
25	20-23 Long Lane	Modern windows to fourth floor	W	21/03/2010	IM
26	3 Lindsey Street	View to rear	W	21/03/2010	IM
27	3 Lindsey Street	Internal view to mezzanine level	NW	21/03/2010	IM
28	3 Lindsey Street	Ceiling detail to mezzanine level	NE	21/03/2010	IM
29	3 Lindsey Street	Ladder to louvre	N	21/03/2010	IM
30	Chimney	External view of Chimney	N	21/03/2010	IM

31	Chimney	View of chimney summit	N	21/03/2010	IM
32	Chimney	View of base of chimney	N	21/03/2010	IM

PHOTOGRAPHIC REGISTER					
FILM NO.	5	NGR	TQ 319 818	FILM TYPE	35mm
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400
Frame No.	Building	Description	Direction	Date	Photographer
1	3 Lindsey Street	View towards track level	S	12/01/2011	SH
2	3 Lindsey Street	Internal view towards lift shaft track level	E	12/01/2011	SH
3	3 Lindsey Street	Aperture in wall track level	W	12/01/2011	SH
4	3 Lindsey Street	Partially blocked up access way track level	N	12/01/2011	SH
5	3 Lindsey Street	Internal view track level	W	12/01/2011	SH
6	3 Lindsey Street	View of barrel vaulted ceiling track level	W	12/01/2011	SH
7	3 Lindsey Street	View of western most room track level	NE	12/01/2011	SH
8	3 Lindsey Street	View of western most room track level	SE	12/01/2011	SH
9	3 Lindsey Street	View of western most room track level	NW	12/01/2011	SH
10	3 Lindsey Street	Corner detail western most room track level	NW	12/01/2011	SH
11	3 Lindsey Street	View of ventilation shaft track level	SE	12/01/2011	SH
12	3 Lindsey Street	View of blocked up apertures basement level	NE	12/01/2011	SH
13	3 Lindsey Street	Barrel vaulted room basement level	SW	12/01/2011	SH
14	3 Lindsey Street	General view from basement level to track level	NW	12/01/2011	SH
15	Retaining wall	General view	N	12/01/2011	SH
16	Retaining wall	Pier detail of northern most section	E	12/01/2011	SH
17	Retaining wall	General view	N	12/01/2011	SH
18	Retaining wall	Pier detail	E	12/01/2011	SH
19	Retaining wall	Cast iron letter 'A'	E	12/01/2011	SH
20	Retaining wall	General view	N	12/01/2011	SH
21	Chimney	Entrance to chimney	E	12/01/2011	SH
22	Chimney	Entrance to chimney with retaining wall to left	E	12/01/2011	SH
23	Chimney	Chimney entrance	E	12/01/2011	SH
24	Chimney	Internal view	-	12/01/2011	SH
25	Chimney	Internal view	-	12/01/2011	SH

PHOTOGRAPHIC REGISTER						
FILM NO.	6	NGR	TQ 319 818	FILM TYPE	35mm	
PROJECT	Farringdon	B&W/COLOUR	B & W	ISO	400	
Frame No.	Building	Description	Direction	Date	Photographer	
1	3 Lindsey Street	General view of site post demolition	S	21/03/2011	SH	
2	3 Lindsey Street	General view of site post demolition	S	21/03/2011	SH	
3	3 Lindsey Street	General view of retaining wall	SE	21/03/2011	SH	
4	3 Lindsey Street	General view of retaining wall	SE	21/03/2011	SH	
5	3 Lindsey Street	General view of retaining wall	E	21/03/2011	SH	
6	3 Lindsey Street	View of lift to base of retaining wall	E	21/03/2011	SH	
7	3 Lindsey Street	Lift detail	E	21/03/2011	SH	
8	3 Lindsey Street	I beam above lift	E	21/03/2011	SH	
9	20-23 Long Lane	View of basemented area (post demolition)	W	21/03/2011	SH	
10	20-23 Long Lane	View of basemented area (post demolition)	S	21/03/2011	SH	
11	20-23 Long Lane	View of basemented area (post demolition)	SE	21/03/2011	SH	
12	20-23 Long Lane	View of splayed lightwell (post demolition)	SW	21/03/2011	SH	
13	20-23 Long Lane	View of basemented area (post demolition)	E	21/03/2011	SH	
14	20-23 Long Lane	View of Vault (P72 on plan)	S	21/03/2011	SH	
15	20-23 Long Lane	View of Vault (P72 on plan)	S	21/03/2011	SH	
16	20-23 Long Lane	View of Vault (P73 on plan)	S	21/03/2011	SH	
17	20-23 Long Lane	View of Vault (P74 on plan)	S	21/03/2011	SH	
18	20-23 Long Lane	View of Vault (P76 on plan)	S	21/03/2011	SH	
19	20-23 Long Lane	View of Vault (P77 on plan)	S	21/03/2011	SH	
20	20-23 Long Lane	View of Vault (P76 on plan)	S	21/03/2011	SH	
21	20-23 Long Lane	View of Vault (P78 on plan)	S	21/03/2011	SH	
22	20-23 Long Lane	View of Vault (P79 on plan)	S	21/03/2011	SH	
23	20-23 Long Lane	View of Vault (P80 on plan)	S	21/03/2011	SH	
24	20-23 Long Lane	View of Vault (P81 on plan)	S	21/03/2011	SH	
25	20-23 Long Lane	View of Vault (P82 on plan)	S	21/03/2011	SH	
26	20-23 Long Lane	View of Vault (P83 on plan)	S	21/03/2011	SH	